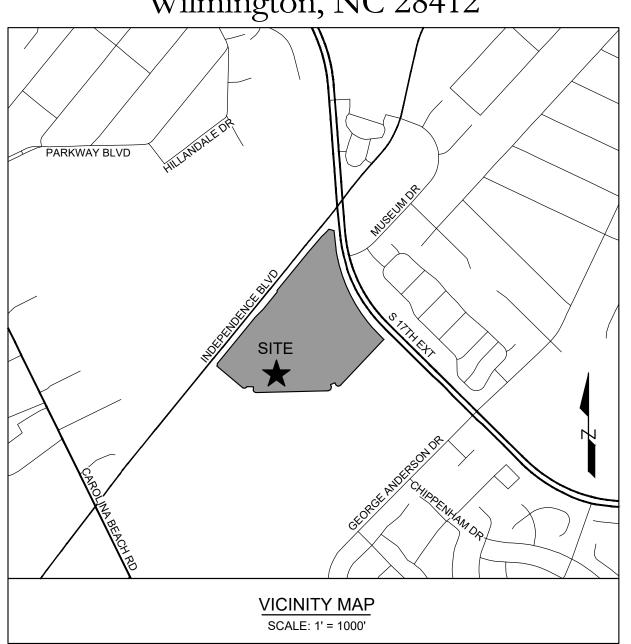
THE POINTE AT BARCLAY PHASE III HOTEL TRACT

WILMINGTON, NORTH CAROLINA

DESIGN DOCUMENTS JANUARY 2020

PROJECT ADDRESS:

2177 Stone Crop Drive Wilmington, NC 28412



NOTICE REQUIRED

ALL EXISTING UNDERGROUND UTILITIES SHALL BE PHYSICALLY LOCATED PRIOR TO THE BEGINNING OF ANY CONSTRUCTION IN THE VICINITY OF SAID UTILITIES.

CONTRACTORS SHALL NOTIFY OPERATORS WHO MAINTAIN UNDERGROUND UTILITY LINES IN THE AREA OF PROPOSED EXCAVATION AT LEAST TWO WORKING DAYS, BUT NOT MORE THAN TEN WORKING DAYS PRIOR TO COMMENCEMENT OF EXCAVATION OR DEMOLITION.

CONTACT "CAROLINA ONE CALL" AT 1-800-632-4949

CONTACT THESE UTILITIES

CITY OF WILMINGTON PLANNING & DEVELOPMENT

ATTN: ZONING INSPECTIONS

PH: 910-342-2782

ATTN: BRIAN CHAMBERS, PLANNER

PH: 910-254-0900

PIEDMONT NATURAL GAS ATTN: PAUL GONKA

PH: 910-251-2810 **EMERGENCY DIAL 911**

POLICE - FIRE - RESCUE ATTN: CITY OF WILMINGTON FIRE & LIFE SAFETY PH: 910-343-0696

CAPE FEAR PUBLIC UTILITY AUTHORITY (WATER & SEWER) **ENGINEERING/INSPECTIONS** PH: 910-332-6560

OPERATIONS/MAINTENANCE

DUKE ENERGY

DISTRIBUTION CONSTRUCTION SERVICE DEP CSC PH: 1-800-452-2777

TRANSMISSION AGENT BILL WILDER PH: 910-772-4903

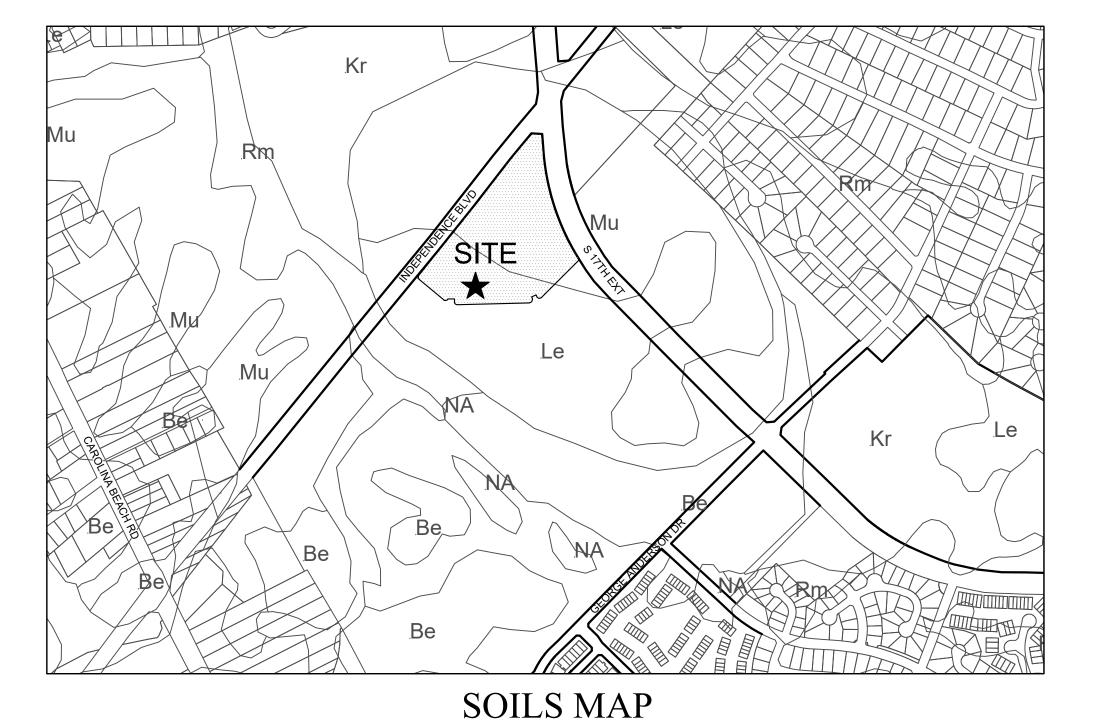
PH: 910-322-6550

AT&T/BELL SOUTH

ATTN: STEVE DAYVAULT (BUILDING ENGINEERING) PH: 910-341-0741

ATTN: JAMES BATSON, ENGINEERING

SPECTRUM GENERAL PH: 800-892-4357



SCALE: 1"=500'

OWNER & DEVELOPER

CIVIL ENGINEERS, LAND PLANNERS & LANDSCAPE ARCHITECTS:

122 CINEMA DRIVE WILMINGTON, NORTH CAROLINA 28403

CIVIL ENGINEER - ATTN: DAN FISK, PE

LANDSCAPE ARCHITECT - ATTN: ALLISON ENGEBRETSON, RLA

| SHEET INDEX | | |
|---------------|-------------------------|--|
| SHEET NUMBER | SHEET TITLE | |
| C-0.0 | COVER SHEET | |
| C-1.0 | GENERAL NOTES | |
| C-1.1 | NPDES NOTES | |
| C-2.0 | OVERALL SITE PLAN | |
| C-2.1 | SITE PLAN | |
| C-3.0 - C-3.1 | EROSION CONTROL PLAN | |
| C-4-0 | GRADING & DRAINAGE PLAN | |
| C-5-0 - C-5.5 | DETAILS | |
| | | |
| | | |
| | | |

PREPARED BY: PARAMOUNTE ENGINEERING.INC.

Wilmington, North Carolina 28403 (910) 791-6707 (O) (910) 791-6760 (F) NC License #: C-2846 PROJECT # 18485.PE

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance.

| | Approved Construction Plan | | |
|-----------|----------------------------|-------------|--|
| | <u>Name</u> | <u>Date</u> | |
| Plannin | g | _ | |
| Traffic _ | | | |
| Fire _ | | | |
| | | | |

WILMINGTON

Wilmington Hotel Investors,

Professional Seal edacted on electron copy per City of Wilmington Policy

- ALL CONSTRUCTION TO BE IN ACCORDANCE WITH PERMITS ISSUED AND WITH THE CITY OF WILMINGTON, NEW HANOVER COUNTY, CAPE
- FEAR PUBLIC UTILITY AUTHORITY (CFPUA). AND THE STATE OF NORTH CAROLINA. THE CONTRACTOR IS TO ESTABLISH AND CHECK ALL HORIZONTAL AND VERTICAL CONTROLS TO BE USED WITH THE PROJECT. IN
- ANYTIME WORK IS PERFORMED OFF-SITE OR WITHIN AN EXISTING FASEMENT. THE CONTRACTOR IS TO NOTICY THE HOLDER OF SAID EASEMENT AS TO THE NATURE OF PROPOSED WORK, AND TO FOLLOW ANY GUIDELINES OR STANDARDS WHICH ARE ASSOCIATED WITH OR

ADDITION. THE CONTRACTOR IS TO COMPUTE THE LAYOUT OF THE ENTIRE SITE PLAN IN ADVANCE OF BEGINNING ANY WORK ASSOCIATED

WITH THE SUBJECT PLANS. CONTRACTOR SHALL EMPLOY A PROFESSIONAL SURVEYOR TO PERFORM SITE IMPROVEMENT STAKEOUT(S).

- CONTRACTOR SHALL REFER TO ARCHITECTURAL DRAWINGS BY HIPP ARCHITECTURE & DEVELOPMENT, PC FOR ALL BUILDING DIMENSIONS AND DETAILS.

GENERAL NOTES:

- TREE INVENTORY AND TOPOGRAPHIC SURVEY COMPLETED BY PARAMOUNTE ENGINEERING, INC. THE SURVEY SHALL BE FIELD VERIFIED BY CONTRACTOR AND ANY DISCREPANCIES REPORTED TO THE OWNER, OWNERS REPRESENTATIVE AND/OR PARAMOUNTE ENGINEERING INC. IMMEDIATELY. GALLERY PARK BOULEVARD UTILITY, LAYOUT AND GRADING INFORMATION WAS TAKEN FROM HANOVER DESIGN SERVICES, P.A. DESIGN PLANS. THE POINTE PHASE I AND PHASE II UTILITY, LAYOUT AND GRADING INFORMATION WAS TAKEN FROM A COMBINATION OF PARAMOUNTE ENGINEERING, INC. DESIGN PLANS AND FIELD SURVEY. CONTRACTOR SHALL VERIFY THAT THE AS-BUILT CONDITIONS MATCH THE DESIGN CONDITIONS PROVIDED FOR ALL UTILITY CONNECTIONS, STORM CONNECTIONS, GRADING TIE-INS, ETC. AND NOTIFY THE OWNER AND ENGINEER OF ANY DISCREPANCIES.
- REASONABLE CARE HAS BEEN EXERCISED IN SHOWING THE LOCATION OF EXISTING UTILITIES ON THE PLANS. THE EXACT LOCATION OF ALL EXISTING UTILITIES IS NOT KNOWN IN ALL CASES. THE CONTRACTOR SHALL EXPLORE THE AREA AHEAD OF DITCHING OPERATIONS BY OBSERVATIONS, ELECTRONIC DEVICES, HAND DIGGING AND BY PERSONAL CONTACT WITH THE UTILITY COMPANIES. IN ORDER TO LOCATE EXISTING UTILITIES IN ADVANCE OF TRENCHING OPERATIONS SO AS TO ELIMINATE OR MINIMIZE DAMAGE TO EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS RESULTING FROM ANY DAMAGE TO THE EXISTING UTILITY LINES INCLUDING LOSS OF UTILITY REVENUES. CONTRACTOR SHALL ARRANGE FOR TEMPORARY SUPPORT OF EXISTING UTILITIES, SUCH AS POLES, CONDUITS, FIBER OPTIC CABLES, TELEPHONE CABLES, WATER LINES, ETC.
- CONTRACTOR SHALL COMPLY WITH THE LATEST REVISIONS AND INTERPRETATIONS OF THE DEPARTMENT OF LABOR SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION PROMULGATED UNDER THE OCCUPATIONAL SAFETY AND HEALTH ACT.
- CONTRACTOR SHALL PLAN AND CONSTRUCT WORK SO AS TO CAUSE MINIMUM INCONVENIENCE TO THE OWNER AND THE PUBLIC. THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN AT ALL TIMES DURING THE PROGRESS OR TEMPORARY SUSPENSION OF WORK, SUITABLE BARRIERS, FENCES, SIGNS OR OTHER ADEQUATE PROTECTION, INCLUDING FLAGMEN AND WATCHMEN AS NECESSARY TO INSURE THE SAFETY OF THE PUBLIC AS WELL AS THOSE ENGAGED IN THE CONSTRUCTION WORK. CONSTRUCTION SIGNING SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF "CONSTRUCTION AND MAINTENANCE OPERATIONS SUPPLEMENT TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" BY THE DOT.
- ALL MATERIAL CLEARED OR DEMOLISHED BY THE CONTRACTOR IN ORDER TO CONSTRUCT THE WORK SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE PROPERLY DISPOSED OF OFF-SITE.
- ALL WORK BY THE CONTRACTOR SHALL BE WARRANTEED BY THE CONTRACTOR FOR A PERIOD OF ONE YEAR AFTER THE OWNER ACCEPTS THE WORK.
- CONTRACTOR SHALL CALL THE NORTH CAROLINA ONE-CALL CENTER AT 1 (800) 632-4949 AN ALLOW THE CENTER TO LOCATE EXISTING
- STREET TREES MUST BE LOCATED A MINIMUM OF 15 FEET FROM STREET LIGHTS.
- PRIOR TO ANY CLEARING, GRADING OR CONSTRUCTION ACTIVITY, TREE PROTECTION FENCING WILL BE INSTALLED AROUND PROTECTED TREES OR GROVE OF TREES. NO CONSTRUCTION WORKERS, TOOLS, MATERIALS, OR VEHICLES ARE PERMITTED WITHIN THE TREE
- ALL PROPOSED VEGETATION WITHIN SIGHT TRIANGLES SHALL NOT INTERFERE WITH CLEAR VISUAL SIGHT LINES FROM 30"-10".

DEMOLITION NOTES:

- CONTRACTOR TO COORDINATE WITH THE OWNER TO PROPERLY MAINTAIN OR RELOCATE EXISTING SERVICE CONNECTIONS WHEN
- CONTRACTOR IS TO WALK THE SITE AND BECOME FAMILIAR WITH THE SCOPE OF DEMOLITION REQUIRED. ALL DEMOLITION WORK REQUIRED TO CONSTRUCT NEW SITE IMPROVEMENTS WILL BE PERFORMED BY THE CONTRACTOR AND WILL BE CONSIDERED UNCLASSIFIED EXCAVATION.
- DEMOLITION SHALL INCLUDE BUT IS NOT LIMITED TO THE EXCAVATION, HAULING AND OFFSITE DISPOSAL OF CONCRETE PADS, CONCRETE DITCHES, FOUNDATIONS, SLABS, STEPS, AND STRUCTURES; ABANDONED UTILITIES, BUILDINGS, PAVEMENTS AND AL MATERIALS CLEARED AND STRIPPED TO THE EXTENT NECESSARY AS DIRECTED BY THE SOILS ENGINEER FOR THE INSTALLATION OF THE NEW IMPROVEMENTS AND WITHIN THE LIMITS OF CLEARING AND GRADING AND AS SHOWN ON THESE PLANS
- THE CONTRACTOR SHALL PROTECT ALL ADJACENT PROPERTY. STRUCTURES AND UTILITIES ON THE PROPERTY NOT TO BE DEMOLISHED DAMAGE TO PROPERTIES OF OTHERS DUE TO THE CONTRACTOR'S ACTIVITIES SHALL BE REPLACED IN KIND BY THE CONTRACTOR AT NO
- ELECTRIC, TELEPHONE, SANITARY SEWER, WATER AND STORM SEWER UTILITIES THAT SERVICE OFF-SITE PROPERTIES SHALL BE MAINTAINED DURING THE CONSTRUCTION PROCESS BY THE CONTRACTOR.
- THE CONTRACTOR SHALL PRODUCE A PHOTOGRAPHIC RECORD (DIGITAL) OF DEVELOPMENT COMMENCING WITH A RECORD OF THE SITE AS IT APPEARS BEFORE DEMOLITION HAS BEGUN. AFTERWARDS, A PHOTOGRAPHIC RECORD SHALL BE MAINTAINED WEEKLY DURING CONSTRUCTION AND ENDING WITH A PHOTOGRAPHIC RECORD OF THE DEVELOPMENT AS IT APPEARS AFTER DEMOLITION. THIS RECORD SHALL BE DELIVERED TO THE OWNER.
- EXISTING CURB AND GUTTER, LIGHTS, SIDEWALK, AND UTILITIES NOT INTENDED FOR DEMOLITION SHALL BE MAINTAINED, PROTECTED AND UNDISTURBED DURING DEMOLITION.
- ALL EXISTING IMPROVEMENTS INDICATED OR REQUIRED TO BE DEMOLISHED SHALL INCLUDE REMOVAL FROM THE PROPERTY AND
- CONTRACTOR SHALL COORDINATE RELOCATION OF ALL EXISTING OVERHEAD AND UNDERGROUND UTILITIES INCLUDING CABLE. GAS. TELEPHONE AND ELECTRIC AND ANY OTHER UTILITIES THROUGH THE SITE WITH THE RESPECTIVE COMPANIES.
- PROVIDE SMOOTH SAW CUT OF EXISTING PAVEMENTS, CURBS AND GUTTERS AND SIDEWALKS TO BE DEMOLISHED.
- ALL DEMOLITION WORK SHALL BE DONE IN STRICT ACCORDANCE WITH LOCAL. STATE AND FEDERAL REGULATIONS AS WELL AS OSHA
- EXISTING FIRE HYDRANTS ON OR NEAR THE SITE ARE TO REMAIN IN SERVICE.
- ILITIES WAS OBTAINED FROM AVAILABLE RECORDS, BUT THE CONTRACTOR MUST DETERMINE THE EXACT LOCATION AND ELEVATIONS OF THE MAINS BY DIGGING TEST PITS BY HAND

RAFFIC NOTES:

- ALL PAVEMENT MARKINGS IN PUBLIC RIGHTS-OF-WAY & FOR DRIVEWAY(S) ARE TO BE THERMOPLASTIC & MEET CITY AND/OR NCDOT
- TRAFFIC CONTROL DEVICES (INCLUDING SIGNS AND PAVEMENT MARKINGS) IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO MEET MUTCD (MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES) STANDARDS.
- ALL TRAFFIC CONTROL SIGNS AND MARKINGS NOT WITHIN THE PUBLIC RIGHT-OF-WAY ARE TO BE MAINTAINED BY THE PROPERTY OWNER IN ACCORDANCE WITH MUTCD STANDARDS.
- ALL PARKING STALL MARKINGS AND LANE ARROWS WITHIN THE PARKING AREAS SHALL BE WHITE.
- ANY OPEN CUTTING OF A CITY STREET REQUIRES A UTILITY CUT PERMIT. CONTACT 341-5899 FOR MORE DETAILS. IN CERTAIN CASES, AN
- CONTACT 811 PRIOR TO CONTACTING CITY OF WILMINGTON, TRAFFIC ENGINEERING REGARDING THE UTILITIES IN ROW. CONTACT TRAFFIC ENGINEERING, AT 341-7888 TO ENSURE THAT ALL TRAFFIC SIGNAL FACILITIES AND EQUIPMENT ARE SHOWN ON THE PLAN. CALL TRAFFIC ENGINEERING FORTY-EIGHT (48) HOURS PRIOR TO ANY EXCAVATION IN THE R/W.
- ANY BROKEN OR MISSING SIDEWALK PANELS, DRIVEWAY PANELS AND/OR CURBING SHALL BE REPLACED
- TACTILE WARNING MATS TO BE INSTALLED AT ALL WHEELCHAIR RAMPS.
- A LANDSCAPING PLAN, INDICATING THE LOCATION OF REQUIRED STREET- YARD PLANTINGS SHALL BE PROVIDED FOR REVIEW OF SIGHT DISTANCES AND ANY OTHER REQUIREMENTS OF TRAFFIC ENGINEERING.
- ALL SIGNS AND PAVEMENT MARKINGS IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO MEET MUTCD STANDARDS.

ROSION CONTROL AND SEQUENCE OF CONSTRUCTION NOTES:

PROTECTION TO STOP ANY SEDIMENT FROM ENTERING THE DRAINAGE SYSTEM

S STABILIZED AND ALL IMPROVEMENTS WITHIN THE BASIN ARE COMPLETE.

NOTE: THESE FROSION CONTROL AND SEQUENCE OF CONSTRUCTION NOTES ARE INTENDED FOR EACH "PHASE" OF CONSTRUCTION. THE ORDER AND STEPS TAKEN MUST BE IMPLEMENTED AS EACH PART OF THE PROJECT IS DEVELOPED; WHETHER AS A WHOLE OR IN PHASES. ANY ROSION CONTROL DEVICES/MEASURES MUST REMAIN IN PLACE UNTIL THE ENTIRE BASIN DESIGNED TO HANDLE SEDIMENTATION AND EROSION

- CONSTRUCT TEMPORARY GRAVEL CONSTRUCTION ENTRANCE(S), ESTABLISH THE LIMITS OF DISTURBANCE, AND TEMPORARY SILT FENCE. CLEAR AND REMOVE FROM SITE ALL TREES (ONLY ONES TO BE REMOVED), ROOTS, ROOT MAT, ETC. FROM THE AREA WITHIN THE
- INSTALL REMAINING EROSION CONTROL MEASURES AS SHOWN ON THE PLANS WITHIN THE AREA DISTURBED. ALL EROSION CONTROL MEASURES MUST BE INSTALLED BEFORE COMMENCING CONSTRUCTION.
- PLANT GRASS OVER ALL GRADED AREAS WITHIN 14 WORKING DAYS OF CEASE OF ANY GRADING ACTIVITY IMMEDIATELY UPON THE INSTALLATION OF ANY STORM DRAINAGE CATCH BASIN, DROP INLET, ETC., THE CONTRACTOR SHALL INSTALL INLET
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING AND RESTORING TO PRE-CONSTRUTION CONDITIONS ANY AREAS OUTSIDE THE PROJECT LIMITS THAT MAY INADVERTENTLY BE DAMAGED DUE TO THE FAILURE OF THE EROSION CONTROL MEASURES.
- DURING GRADING AND AFTER GRADING HAS BEEN COMPLETED, THE CONTRACTOR SHALL CONTINUE TO MAINTAIN PERMANENT AND
- TEMPORARY EROSION CONTROL MEASURES UNTIL FINAL APPROVAL UPON RECEIVING FINAL APPROVAL, THE CONTRACTOR CAN REMOVE TEMPORARY EROSION CONTROL MEASURES.
- THE CONTRACTOR SHALL CONTINUE TO WATER, FERTILIZE, MOW AND MAINTAIN SPRIGGED, SODDED, AND PLANTED AREAS UNTIL ALL

SENERAL EROSION AND SEDIMENT CONTROL NOTES:

- THE EROSION CONTROL PLAN SHALL INCLUDE PROVISIONS FOR GROUNDCOVER ON ALL EXPOSED PERIMETER DIKES, SWALES, DITCHES PERIMETER SLOPES AND ALL SLOPES STEEPER THAN 3:1 WITHIN 7 CALENDAR DAYS FROM THE LAST LAND DISTURBING ACTIVITY. GROUND COVER SHALL BE PROVIDED ON ALL OTHER DISTURBED AREAS WITHIN 14 CALENDAR DAYS FROM THE LAST LAND DISTURBING ACTIVITY.
- UNLESS OTHERWISE INDICATED. ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE NORTH CAROLINA EROSION AND SEDIMENT CONTROL HANDBOOK. (NO SEPARATE PAYMENT).
- THE CONTRACTOR SHALL NOTIFY PLAN APPROVING AUTHORITY ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITY, AND ONE WEEK PRIOR TO FINAL INSPECTION. ALL EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE PLACED PRIOR TO CLEARING AND/OR LAND DISTURBANCE.
- A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN AND PERMIT SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- PRIOR TO COMMENCING LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING, BUT NOT LIMITED TO OFF-SITE BORROW OR WASTE AREAS STAGING OR STORAGE AREAS), THE CONTRACTOR SHALL PREPARE AND SUBMIT A SUPPLEMENTARY EROSION CONTROL PLAN TO THE OWNER FOR REVIEW AND TO NEW HANOVER COUNTY FOR APPROVAL. CONTRACTOR SHALL PAY ALL FEES REQUIRED AND SHALL INSTALL NECESSARY MEASURES AT NO SEPARATE PAYMENT. THE CONTRACTOR SHALL PROVIDE THE OWNER AND THE ENGINEER A COPY OF THE AMENDED PERMIT
- THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY EITHER THE REVIEWING AGENCY OR THE ENGINEER. (NO SEPARATE PAYMENT).
- ALL DISTURBED AREAS ARE TO DRAIN TO APPROVED SEDIMENT CONTROL MEASURES AT ALL TIMES DURING LAND DISTURBING ACTIVITIES AND DURING SITE DEVELOPMENT UNTIL FINAL STABILIZATION IS ACHIEVED
- ALL AREAS DISTURBED BY CONSTRUCTION UNLESS OTHERWISE IMPROVED SHALL BE SODDED OR SEEDED AS INDICATED AND STABILIZED. DURING DEWATERING OPERATIONS, WATER SHALL BE PUMPED INTO AN APPROVED FILTERING DEVICE PRIOR TO DISCHARGE TO RECEIVING

GENERAL EROSION AND SEDIMENT CONTROL NOTES (CONT.)

PLAN TO MAINTAIN A VIGOROUS, DENSE VEGETATIVE COVER.

- 11. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES SHALL BE MADE IMMEDIATELY
- ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED BY CONTRACTOR ONCE STABILIZATION OR A SUFFICIENT GROUND COVER HAS BEEN ESTABLISHED OR AS DIRECTED BY THE ENGINEER. (NO SEPARATE PAYMENT). NEW HANOVER COUNTY'S FINAL
- TEMPORARY GRAVEL CONSTRUCTION ENTRANCE SHALL BE REQUIRED AT ALL CONSTRUCTION STAGING AREA ENTRANCES AND ALL CONSTRUCTION ACCESS LOCATIONS INTO NON-PAVED AREA. (NO SEPARATE PAYMENT)
- WHEN CROSSING CREEK OR DRAINAGE-WAY, THE DIVISION OF WATER QUALITY SHALL BE CONTACTED PRIOR TO DISTURBING A CREEK THE CONTRACTOR SHALL INSTALL RIP-RAP WITH FABRIC ALONG DISTURBED BANKS AND CHANNEL AND RESTORE SLOPES TO ORIGINAL CONTOURS, BUT NOT STEEPER THAN 2:1 MAXIMUM. DISTURBED CREEK AREA SHALL BE STABILIZED IMMEDIATELY

MAINTENANCE PLAN:

- 1. ALL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CHECKED FOR STABILITY AND OPERATION FOLLOWING EVERY RUNOFF-PRODUCING RAINFALL BUT IN NO CASE LESS THAN ONCE EVERY WEEK. ANY NEEDED REPAIRS WILL BE MADE IMMEDIATELY TO MAINTAIN ALL PRACTICES AS DESIGNED.
- ALL CONSTRUCTION ENTRANCES WILL BE PERIODICALLY TOP DRESSED WITH AN ADDITIONAL 2 INCHES OF #4 STONE TO MAINTAIN PROPER
- SEDIMENT WILL BE REMOVED BEHIND THE SEDIMENT FENCE WHEN IT BECOMES 0.5 FEET DEEP AT THE FENCE. THE SEDIMENT FENCE WILL BE BE REPAIRED AS NECESSARY TO MAINTAIN A BARRIER. SILT FENCE STAKES WILL BE SPACED 6 FEET APART UNLESS A WIRE BACKING IS
- ALL SEEDED AREAS WILL BE FERTILIZED, RESEEDED AS NECESSARY, AND MULCHED ACCORDING TO SPECIFICATIONS IN THE VEGETATIVE
- INLET PROTECTION INSPECT INLETS AT LEAST WEEKLY AND AFTER EACH SIGNIFICANT (\$\frac{1}{2}\text{ INCH OR GREATER}) RAINFALL EVENT. CLEAR THE MESH WIRE OF ANY DEBRIS OR OTHER OBJECTS TO PROVIDE ADEQUATE FLOW FOR SUBSEQUENT RAINS. TAKE CARE NOT TO DAMAGE OR UNDERCUT THE WIRE MESH DURING SEDIMENT REMOVAL. REPLACE STONE AS NEEDED
- SILT SACK INLET PROTECTION INSPECT INLETS AT LEAST WEEKLY AND AFTER EACH RAINFALL EVENT. REMOVE DEBRIS OR OTHER
- SEDIMENT BASIN/SEDIMENT TRAPS REMOVE SEDIMENT AND RESTORE THE BASIN TO ITS ORIGINAL DIMENSIONS WHEN IT ACCUMULATES O WITHIN ONE HALF OF THE DESIGN DEPTH. PLACE SEDIMENT IN AREA WITH SEDIMENT CONTROLS. CHECK THE EMBANKME SPILLWAYS, AND OUTLET FOR EROSION DAMAGE, PIPING, AND SETTLEMENT. MAKE ALL NECESSARY REPAIRS IMMEDIATELY. REMOVE ALL
- ASH AND OTHER DEBRIS FROM THE RISER AND POOL AREA. SKIMMER - INSPECT SKIMMER AT LEAST ONCE A WEEK AND AFTER EACH RAINFALL TO MAKE SURE THAT THE INTAKE MECHANISM, ORIFICE, OR DISCHARGE PIPE IS NOT CLOGGED WITH TRASH OR SEDIMENT. IF THE BASIN IS DRY, MAKE SURE THAT ANY VEGETATION GROWING ON THE BOTTOM IS NOT HOLDING THE SKIMMER DOWN. TAKE SPECIAL PRECAUTION IN WINTER TO PREVENT THE SKIMMER FROM PLUGGING
- OUTLET PROTECTION INSPECT RIP RAP OUTLET STRUCTURES WEEKLY AND AFTER SIGNIFICANT (\$\frac{1}{2}\$ INCH OR GREATER) RAINFALL EVENTS O SEE IF ANY EROSION AROUND OR BELOW THE RIP RAP HAS TAKEN PLACE, OR IF STONES HAVE BEEN DISLODGED. IMMEDIATELY MAKE ALL NEEDED REPAIRS TO PREVENT FURTHER DAMAGE.
- 10. EMERGENCY SPILLWAY / FOREBAY PROTECTION AFTER EVERY HIGH-WATER EVENT INSPECT THE INTEGRITY OF THE LINED SPILLWAY AND IE ADJACENT EARTHEN BANKS. IMMEDIATELY MAKE ALL NEEDED REPAIRS TO PREVENT FURTI RIP RAP LINED APRONS. RE-ESTABLISH ANY LOOSE STONES, AND FIX GAPS IN THE ADJACENT VEGETATIVE COVER.
- GRASS SWALES INSPECT SWALES WEEKLY AND AFTER EVERY RAINFALL EVENT. AFTER GRASS HAS BEEN ESTABLISHED INSPECT THE SWALES PERIODICALLY AND AFTER EVERY HEAVY (\$\frac{1}{2}\) INCH OR GREATER) RAINFALL EVENT AND REPAIR IMMEDIATELY. INSPECT THE CHANNEL OUTLET AND ALL ROAD CROSSINGS FOR BANK STABILITY AND EVIDENCE OF PIPING OR SCOUR HOLES. REMOVE ALL SIGNIFICANT SEDIMENT ACCUMULATIONS TO MAINTAIN THE DESIGNED CARRYING CAPACITY. KEEP THE GRASS IN A HEALTHY, VIGOROUS CONDITION AT ALL TIMES, SINCE IT IS THE PRIMARY EROSION PROTECTION FOR THE CHANNEL.
- CONCRETE WASHOUTS CONCRETE WASHOUTS SHOULD BE INSPECTED DAILY AND AFTER HEAVY RAINS. DAMAGES SHOULD BE REPAIRED PROMPTLY. IF FILLED TO OVER 75% CAPACITY WITH RAIN WATER IT SHOULD BE VACUUMED OR ALLOWED TO EVAPORATE TO AVOID OVERFLOWS. BEFORE HEAVY RAINS THE CONTAINERS LIQUID LEVEL SHOULD BE LOWERED OR THE CONTAINER COVERED TO AVOID AN PERMANENT SEEDING SCHEDULE - MIXTURE 5CP

RATE (LB/ACRE)

10-20

Seeding Mixture SPECIES

CENTIPEDE GRASS

Seeding Dates MARCH - JUNE

APPLY LIME AND FERTILIZER ACCORDING TO SOIL TESTS, OR APPLY 300 LB/ACRE 10-10-10 FERTILIZER.

DO NOT MULCH.

FERTILIZE VERY SPARINGLY--20 LB/ACRE NITROGEN IN SPRING WITH NO PHOSPHORUS. CENTIPEDE GRASS CANNOT TOLERATE HIGH PI OR EXCESS FERTILIZER.

TEMPORARY SEEDING-LATE WINTER/EARLY SPRING

WINTER AND EARLY SPRING

| SPECIES | RATE (LB/ACRE) |
|--|----------------|
| RYE (GRAIN) | 120 |
| ANNUAL LESPEDEZA (KOBE IN PIEDMONT AND COASTAL PLAIN KOREAN IN MOLINTAINS) | 50 |

| PLAIN, KOREAN IN MOUNTAINS) | 50 |
|--|----|
| T ANNUAL LESPEDEZA WHEN DURATION OF TEMPORARY COVER IS NOT TO EXTEND OND JUNE. | |
| SUMMER | |

| SPECIES | RATE (LB/AC |
|---------------|-------------|
| GERMAN MILLET | 40 |

RATE (LB/ACRE) RYE (GRAIN)

Seeding dates

COASTAL PLAIN - DEC.1-APR. 15 - LATE WINTER AND EARLY SPRING APRIL 15 - AUG. 15 - SUMMER AUG. 15 - DEC. 30 - FALL

Soil amendments

FOLLOW RECOMMENDATIONS OF SOIL TESTS OR APPLY 2,000 LB/ACRE GROUND AGRICULTURAL LIMESTONE AND 750 LB/ACRE 10-10-10 FERTILIZER.

APPLY 4,000 LB/ACRE STRAW. ANCHOR STRAW BY TACKING WITH ASPHALT, NETTING OR A MULCH ANCHORING TOOL. A DISK WITH BLADES SET NEARLY STRAIGHT CAN BE USED AS A MULCH ANCHORING TOOL.

Maintenance REFERTILIZE IF GROWTH IS NOT FULLY ADEQUATE. RESEED, REFERTILIZE AND MULCH IMMEDIATELY FOLLOWING EROSION OR OTHER

SITE NOTES ANY DISCREPANCY IN THIS PLAN AND ACTUAL FIELD CONDITIONS SHALL BE REPORTED TO THE OWNER PRIOR TO START OF CONSTRUCTION. GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF ALL SETBACKS, EASEMENTS AND DIMENSIONS SHOWN HEREON BEFORE BEGINNING CONSTRUCTION.

- CONTRACTOR SHALL MAINTAIN THE SITE IN A MANNER SO THAT WORKMEN AND PUBLIC SHALL BE PROTECTED FROM INJURY, AND ADJOINING PROPERTY PROTECTED FROM DAMAGE.
- ACCESS TO UTILITIES, FIRE HYDRANTS, STREET LIGHTING, ETC., SHALL REMAIN UNDISTURBED, UNLESS COORDINATED WITH THE RESPECTIVE UTILITY.
- DO NOT SCALE THIS DRAWING AS IT IS A REPRODUCTION AND SUBJECT TO DISTORTION. THE GENERAL CONTRACTOR SHALL REMOVE ALL DEBRIS FROM THE SITE UPON COMPLETION OF THE PROJECT AND AT LEAST ONCE A
- WEEK DURING CONSTRUCTION. THE GENERAL CONTRACTOR SHALL KEEP THE AREA OUTSIDE THE "CONSTRUCTION LIMITS" BROOM CLEAN AT ALL TIMES.
- ALL UTILITIES TO SERVICE PROPOSED STRUCTURE SHALL BE UNDERGROUND ON SITE ALL STREET SURFACES, DRIVEWAYS, CULVERTS, CURB AND GUTTERS, ROADSIDE DRAINAGE DITCHES AND OTHER STRUCTURES THAT ARE DISTURBED OR DAMAGED IN ANY MANNER AS A RESULT OF CONSTRUCTION SHALL BE REPLACED OR REPAIRED IN ACCORDANCE WITH
- CONTRACTOR SHALL MAINTAIN AN "AS-BUILT" SET OF DRAWINGS TO RECORD THE EXACT LOCATION OF ALL PIPING PRIOR TO CONCEALMENT. DRAWINGS SHALL BE GIVEN TO THE OWNER UPON COMPLETION OF THE PROJECT WITH A COPY OF THE TRANSMITTAL LETTER TO THE ENGINEER.
- IF DEPARTURES FROM THE SPECIFICATIONS OR DRAWINGS ARE DEEMED NECESSARY BY THE CONTRACTOR, DETAILS OF SUCH DEPARTURES AND REASONS THEREOF SHALL BE GIVEN TO THE OWNER FOR REVIEW. NO DEPARTURES FROM THE CONTRACT DOCUMENT SHALL BE MADE WITHOUT THE PERMISSION OF THE OWNER, THE CITY OF WILMINGTON, NEW HANOVER COUNTY, OR CFPUA,
- CONTRACTOR SHALL VERIFY LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES. THE LOCATION OF ALL EXISTING UTILITIES ARE NOT NECESSARILY SHOWN ON PLANS AND WHERE SHOWN ARE ONLY APPROXIMATE. THE CONTRACTOR SHALL ON HIS INITIATIVE AND AT NO EXTRA COST HAVE LOCATED ALL UNDERGROUND LINES AND STRUCTURES AS NECESSARY. NO CLAIMS FOR DAMAGES OR EXTRA COMPENSATION SHALL ACCRUE TO THE CONTRACTOR FROM THE PRESENCE OF SUCH PIPE OTHER OBSTRUCTIONS OR FROM DELAY DUE O REMOVAL OR REARRANGEMENT OF THE SAME. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UNDERGROUND STRUCTURES. CONTACT NORTH CAROLINA ONE CALL" TOLL FREE 1-800-632-4949 AT LEAST 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL NONSUBSCRIBING UTILITIES.
- THE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL INSPECTIONS, CERTIFICATIONS, EQUIPMENT, ETC., THAT MAY BE REQUIRED. THE ENGINEER AND/OR OWNER DISCLAIM ANY ROLE IN THE CONSTRUCTION MEANS AND METHODS ASSOCIATED WITH THE PROJECT AS
- ALL LOT STRIPING AND DIRECTIONAL ARROWS TO BE REFLECTIVE MARKINGS AND SHALL CONFORM TO NCDOT. ALL PARKING STALL MARKINGS AND LANE ARROWS WITHIN THE PARKING AREAS SHALL BE WHITE.
- LANDSCAPE PLANTINGS AT ENTRANCE/ EXITS WILL BE INSTALLED AND MAINTAINED SO AS NOT TO INTERFERE WITH SIGHT DISTANCE NEEDS OF DRIVERS IN THE PARKING AREA AND AT ENTRANCE/EXIT LOCATIONS PER LOCAL STANDARDS.

ALL PARKING LOT DIMENSIONS ARE FROM FACE OF CURB UNLESS NOTED OTHERWISE.

- SEEDING TO BE INSTALLED TO LOCAL REQUIREMENTS & STANDARD PRACTICES. ALL DIMENSIONS AND RADII ARE TO OUTSIDE FACE OF BUILDING OR TO FACE OF CURB UNLESS OTHERWISE NOTED.
- PROPERTY OWNER'S ASSOCIATION (POA) WILL BE RESPONSIBLE FOR ALL PRIVATE RIGHT OF WAY AND LANDSCAPE MAINTENANCE WITHIN THE PROJECT.

NC ACCESSIBILITY NOTES:

CURB RAMP NOTES:

- IERAL NOTES:
 SPECIAL ATTENTION SHALL BE GIVEN TO COMPLIANCE WITH AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NORTH CAROLINA BUILDING CODE/ANSI A117.1, AND APPLICABLE LOCAL LAWS & REGULATIONS.
- PARAMOUNTE ENGINEERING HAS DEVELOPED THESE NOTES AND DETAILS TO ASSURE THAT CONTRACTORS ARE AWARE OF THE REQUIREMENTS AT THE POINT IN TIME WHEN THEYARE BIDDING THE PROJECT. IN ADDITION, PARAMOUNTE ENGINEERING HAS MADE A POINT IN THESE NOTES AND DETAILS, AS WELL AS IN OUR DRAWINGS, TO PROVIDE SLOPES / GRADES AND DIMENSIONS THAT COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NORTH CAROLINA BUILDING CODE/ANSI A117.1 AND APPLICABLE LOCAL
- AWS & REGULATIONS. IF THESE SLOPES / GRADES AND DIMENSIONS ARE NOT ACHIEVABLE, THE CONTRACTOR IS REQUIRED TO CONTACT THE OWNER IMMEDIATELY AND BEFORE MOVING FORWARD WITH THE WORK.
- THE CONTRACTOR SHALL NOTIFY PARAMOUNTE ENGINEERING IMMEDIATELY OF ANY CONFLICT BETWEEN THESE NOTES AND DETAILS AND OTHER PROJECT DRAWINGS, WHETHER BY PARAMOUNTE ENGINEERING OR OTHERS. THE CONTRACTOR SHALL NOT PROCEED WITH THE WORK FOR WHICH THE ALLEGED CONFLICT HAS BEEN DISCOVERED UNTIL SUCH ALLEGED CONFLICT HAS BEEN RESOLVED. NO CLAIM SHALL BE MADE BY THE CONTRACTOR FOR DELAY OR DAMAGES AS A RESULT OF RESOLUTION OF ANY SUCH CONFLICT(S).
- 4. THESE ACCESSIBILITY NOTES AND DETAILS ARE INTENDED TO DEPICT SLOPE AND DIMENSIONAL REQUIREMENTS ONLY. REFER TO SIDEWALK, CURBING, AND PAVEMENT DETAILS FOR ADDITIONAL INFORMATION.
- AT LEAST ONE ACCESSIBLE ROUTE SHALL BE PROVIDED WITHIN THE SITE FROM ACCESSIBLE PARKING SPACES AND ACCESSIBLE LOADING ZONES; PUBLIC STREETS OR SIDEWALKS; AND PUBLIC TRANSPORTATION STOPS TO THE ACCESSIBLE BUILDING OR
- AT LEAST ONE ACCESSIBLE ROUTE SHALL CONNECT ACCESSIBLE BUILDINGS, ACCESSIBLE FACILITIES, ACCESSIBLE ELEMENTS, AND
- ACCESSIBLE SPACES THAT ARE ON THE SAME SITE. 3. WALKING SURFACES THAT ARE PART OF AN ACCESSIBLE ROUTE SHALL HAVE A MAXIMUM RUNNING SLOPE OF 5.0% AND A MAXIMUM CROSS
- 4. ANY WALKING SURFACE THAT IS PART OF AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 5.0% IS A RAMP AND SHALL COMPLY WITH THE GUIDELINES FOR RAMPS OR CURB RAMPS. TRANSITIONS BETWEEN RAMPS, WALKS, LANDINGS, GUTTERS OR STREETS SHALL BE FLUSHAND FREE OF ABRUPT VERTICAL CHANGES (1/4 INCH MAXIMUM VERTICAL CHANGE IN LEVEL PERMITTED).
- 6. FLOOR SURFACES SHALL BE STABLE, FIRM AND SLIP RESISTANT. 7. THE MINIMUM CLEAR WIDTH OF EXTERIOR ACCESSIBLE ROUTES SHALL BE FORTY-EIGHT (48) INCHES MINIMUM MEASURED BETWEEN HANDRAILS WHERE HANDRAILS ARE PROVIDED (NC BUILDING CODE 1104.1 & 1104.2).
- 8. WHERE AN ACCESSIBLE ROUTE MAKES A 180 DEGREE TURN AROUND AN OBJECT THAT IS LESS THAN FORTY-EIGHT (48) INCHES IN WIDTH CLEAR WIDTH SHALL BE FORTY-TWO (42) INCHES MINIMUM APPROACHING THE TURN. FORTY-EIGHT (48) INCHES MINIMUM DURING THE TURN AND FORTY-TWO (42) INCHES MINIMUM LEAVING THE TURN. THE CLEAR WIDTH APPROACHING AND LEAVING THE TURN MAY BE THIRTY-SIX (36) INCHES MINIMUM WHEN THE CLEAR WIDTH AT THE TURN IS SIXTY (60) INCHES MINIMUM. * SEE NOTE 7 ABOVE FOR NC CLEAR WIDTH OF EXTERIOR ACCESSIBLE ROUTES*
- . AN ACCESSIBLE ROUTE WITH A CLEAR WIDTH LESS THAN SIXTY (60) INCHES SHALLPROVIDE PASSING SPACES AT INTERVALS OF TWO HUNDRED (200) FEET MAXIMUM. PASSING SPACES SHALL BE EITHER A SIXTY (60) INCH MINIMUM BY SIXTY (60) INCH MINIMUM SPACE; OR AN THE T-SHAPED SPACE EXTEND FORTY-EIGHT (48) INCHES MINIMUM BEYOND THE INTERSECTION.
- 10. DOORS, DOORWAYS AND GATES THAT ARE PART OF AN ACCESSIBLE ROUTE SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT 2010 ADA STANDARDS), THE NORTH CAROLINA BUILDING CODE/ ANSI A117.1, AND APPLICABLE LOCAL LAWS & REGULATIONS.

DIRECTIONAL SIGNAGE INDICATING THE ROUTE TO THE NEAREST ACCESSIBLE BUILDING ENTRANCE SHALL BE PROVIDED AT INACCESSIBLE

- 12. WHERE POSSIBLE, DRAINAGE INLETS SHALL NOT BE LOCATED ON AN ACCESSIBLE ROUTE. IN THE EVENT THAT A DRAINAGE INLET MUST BE OCATED ON AN ACCESSIBLE ROUTE, THE GRATE SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS). A117.1, THE NC BUILDING CODE, AND APPLICABLE LOCAL LAWS & REGULATIONS
- ANY PART OF AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 5% SHALL BE CONSIDERED A RAMP. 2. THE MAXIMUM RUNNING SLOPE FOR A RAMP SHALL BE 8.33% AND THE MAXIMUM CROSS SLOPE SHALL BE 2.0%
- 3. THE CLEAR WIDTH OF AN EXTERIOR RAMP RUN SHALL BE FORTY EIGHT INCHES (NC BUILDING CODE 1104.1). WHERE HANDRAILS ARE PROVIDED ON THE RAMP RUN, THE CLEAR WIDTH SHALL BE MEASURED BETWEEN THE HANDRAILS. 4. THE RISE FOR ANY RAMP RUN SHALL BE THIRTY (30) INCHES MAXIMUM.
- DIRECTION. THE LANDING CLEAR WIDTH SHALL BE AT LEAST AS WIDE AS THE WIDEST RAMP RUN LEADING TO THE LANDING. THE LANDING CLEAR LENGTH SHALL BE SIXTY (60) INCHES LONG MINIMUM. RAMPS THAT CHANGE DIRECTION BETWEEN RUNS AT LANDINGS SHALL HAVE A CLEAR LANDING OF SIXTY (60) INCHES BY SIXTY (60) INCHES MINIMUM. 6. RAMP RUNS WITH A RISE GREATER THAN SIX (6) INCHES SHALL HAVE HANDRAILS ON BOTH SIDES COMPLYING WITH THE AMERICANS WITH

5. LANDINGS SHALL BE PROVIDED AT THE TOP AND BOTTOM OF RAMPS. LANDINGS SHALL HAVE A SLOPE NOT STEEPER THAN 2.0% IN ANY

- ABILITIES ACT (2010 ADA STANDARDS), THE NC BUILDING CODE/ANSI A117.1, AND APPLICABLE LOCAL LAWS & REGULATIONS. 7. FLOOR SURFACES OF RAMPS AND LANDINGS SHALL BE STABLE, FIRM AND SLIP RESISTANT.
- 8. EDGE PROTECTION COMPLYING WITH AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NC BUILDING CODE/ANSI A117.1, AND PPLICABLE LOCAL LAWS & REGULATIONS SHALL BE PROVIDED ON EACH SIDE OF RAMP RUNS AND ON EACH SIDE OF RAMP LANDINGS. 9. WHERE DOORWAYS ARE LOCATED ADJACENT TO A RAMP LANDING, MANEUVERING CLEARANCES REQUIRED BY THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NC BUILDING CODE/ANSI A117.1 SHALL BE PERMITTED TO OVERLAP THE REQUIRED LANDING AREA. WHERE DOORS THAT ARE SUBJECT TO LOCKING ARE ADJACENT TO A RAMP LANDING, LANDINGS SHALL BE SIZED TO PROVIDE A COMPLIANT TURNING SPACE.
- 1. THE MAXIMUM RUNNING SLOPE OF A CURB RAMP SHALL BE 8.33% AND THE MAXIMUM CROSS SLOPE SHALL BE 2.0%.
- 2. COUNTER SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE CURB RAMP SHALL NOT BE STEEPER THAN 5%. THE ADJACENT SURFACES AT TRANSITIONS AT CURB RAMPS TO WALKS, GUTTERS AND STREETS SHALL BE AT THE SAME LEVEI 3. THE CLEAR WIDTH OF A CURB RAMP SHALL BE 36 INCHES (36) MINIMUM, EXCLUSIVE OF FLARED SIDES, IF PROVIDED. *NOTE NC BUILDING
- DDE REQUIRES EXTERIOR ACCESSIBLE ROUTES TO BE 48 INCHES MINIMUM WIDE (1104.1 & 1104.2). 4. LANDINGS SHALL BE PROVIDED AT THE TOP OF CURB RAMPS. THE CLEAR LENGTH OF THE LANDING SHALL BE THIRTY-SIX (36) INCHES MINIMUM. THE CLEAR WIDTH OF THE LANDING SHALL BE AT LEAST AS WIDE AS THE CURB RAMP, EXCLUDING FLARED SIDES, LEADING TO THE LANDING. LANDINGS SHALL HAVE A SLOPE NOT STEEPER THAN 2% IN ANY DIRECTION.
- 5. IF A CURB RAMP IS LOCATED WHERE PEDESTRIANS MUST WALK ACROSS THE RAMP, OR WHERE IT IS NOT PROTECTED BY HANDRAILS OR ARDRAILS, IT SHALL HAVE FLARED SIDES. 6. WHERE PROVIDED, CURB RAMP FLARES SHALL NOT EXCEED 10%.

7. CURB RAMPS AND THE FLARED SIDES OF CURB RAMPS SHALL BE LOCATED SO THAT THEY DO NOT PROJECT INTO VEHICULAR TRAFFIC

- NES, PARKING SPACES OR PARKING ACCESS AISLES. CURBS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS EXCLUDING ANY FLARED SIDES 8. CURB RAMPS SHALL BE LOCATED OR PROTECTED TO PREVENT THEIR OBSTRUCTION BY PARKED VEHICLES. 9. IT IS RECOMMENDED TO PROVIDE CURB RAMPS WITH A TWENTY-FOUR (24) INCH DEEP DETECTABLE WARNING COMPLYING WITH 406.12 A117.1, EXTENDING THE FULL WIDTH OF THE RAMP. REFERTO DETECTABLE WARNING DETAILS AND NOTES FOR PLACEMENT, ORIENTATION
- AND NOTES. THE NC BUILDING CODE DOES NOT CURRENTLY REQUIRE DETECTABLE WARNINGS AT CURB RAMPS, NOR DO THE 2010 ADA STANDARDS - HOWEVER US DOT ADA REGULATIONS DO REQUIRE THESE. 10. FLOOR SURFACES OF CURB RAMPS SHALL BE DEEP GROOVED, ½ INCH WIDE BY ¼ INCH DEEP, ONE (1) INCH CENTERS TRANSVERSE TO THE
- 11. WHERE PROVIDED, STOP LINES SHALL BE LOCATED IN ADVANCE OF CURB RAMP.
- 12. WHERE PROVIDED, PEDESTRIAN ACTIVATED SIGNALS SHALL BE LOCATED ADJACENT TO THE SIDEWALK AND NOT ON THE SIDEWALK. 13. WHERE PROVIDED, DRAINAGE INLETS SHALL BE LOCATED UPSTREAM OF CURB RAMPS AND NOT IN THE RAMP AREA.
- 14. CURB RAMP TYPE AND LOCATION ARE PER PLAN. PARKING SPACE NOTES: 1. ACCESSIBLE PARKING SPACES SHALL BE LOCATED ON THE SHORTEST ACCESSIBLE ROUTES OF TRAVEL FROM ADJACENT PARKING TO AN
- 2. ACCESSIBLE PARKING SPACES SHALL BE AT LEAST NINETY-SIX (96) INCHES WIDE. ACCESS AISLES SHALL BE 60 INCHES WIDE. ONE OF SIX ACCESSIBLE SPACES SHOULD PROVIDE A VAN ACCESSIBLE AISLE. THE AISLE SHOULD BE 96 INCHES WIDE (OR ACCESSIBLE SPACE IS 11 FEET AND ACCESS AISLE IS FIVE FEET). WHERE PARKING SPACES AND ACCESS AISLES ARE MARKED WITH LINES, THE WIDTH MEASUREMENTS SHALL BE MADE FROM CENTERLINE OF THE MARKINGS. WHERE PARKING SPACES OR ACCESS AISLES ARE NOT ADJACENT TO ANOTHE PARKING SPACE OR ACCESS AISLES, MEASUREMENTS SHALL BE PERMITTED TO INCLUDE THE FULL WIDTH OF THE LINE DEFINING THE
- 3. PARKING ACCESS AISLES SHALL BE PART OF AN ACCESSIBLE ROUTE TO THE BUILDING OR FACILITY ENTRANCE AND SHALL COMPLY WITH PROVISIONS FORACCESSIBLE ROUTES. MARKED CROSSINGS SHALL BE PROVIDED WHERE THE ACCESSIBLE ROUTE MUST CROSS VEHICULAR TRAFFIC LANES. WHERE POSSIBLE, IT IS PREFERABLE THAT THE ACCESSIBLE ROUTE NOT PASS BEHIND PARKED VEHICLES.
- 4. TWO (2) ACCESSIBLE PARKING SPACES MAY SHARE A COMMON ACCESS AISLE 5. ACCESS AISLES SHALL EXTEND THE FULL LENGTH OF THE PARKING SPACE THEY SERVE.

BUILDING CODE/ANSI A117.1 AND SHALL BE ON AN ACCESSIBLE ROUTE

- 6. ACCESS AISLES SHALL BE MARKED SO AS TO DISCOURAGE PARKING IN THEM 7. ACCESS AISLES SHALL NOT OVERLAP THE VEHICULAR WAY. ACCESS AISLES SHALL BE PERMITTED TO BE PLACED ON EITHER SIDE OF THE
- PARKING SPACE EXCEPTFOR ANGLED VAN PARKING SPACES WHICH SHALL HAVE ACCESS AISLES LOCATED ON THE PASSENGER SIDE OF THE 8. FLOOR SURFACES OF PARKING SPACES AND ACCESS AISLES SERVING THEM SHALL BE STABLE, FIRM AND SLIP RESISTANT. ACCESS AISLES SHALL BE AT THE SAME LEVEL AS THE PARKING SPACES THEY SERVE. CHANGES IN LEVEL ARE NOT PERMITTED.

9. PARKING SPACES AND ACCESS AISLES SHALL BE LEVEL WITH SURFACE SLOPES NOT EXCEEDING 2.0% IN ALL DIRECTIONS.

- 10. PARKED VEHICLE OVERHANGS SHALL NOT REDUCE THE REQUIRED CLEAR WIDTH OF AN ACCESSIBLE ROUTE 11. PARKING SPACES FOR VANS AND ACCESS AISLES AND VEHICULAR ROUTES SERVING THEM SHALL PROVIDE A VERTICAL CLEARANCE OF NINETY-EIGHT (98) INCHES MINIMUM. SIGNS SHALL BE PROVIDED AT ENTRANCES TO PARKING FACILITIES INFORMING DRIVERS OF CLEARANCES AND THE LOCATION OF VAN ACCESSIBLE PARKING SPACES.
- 12. EACH ACCESSIBLE PARKING SPACE SHALL BE PROVIDED WITH SIGNAGE DISPLAYING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY. SIGNS SHALL BE INSTALLED AT A MINIMUM CLEAR HEIGHT OF SIXTY (60) INCHES ABOVE GRADE AND SHALL NOT INTERFERE WITH AN ACCESSIBLE ROUTE FROM AN ACCESS AISLE. SIGNS LOCATED WHERE THEY MAY BE HIT BY VEHICLES BEING PARKED SHALL BE INSTALLED WITH BOLLARD 13. SIGNAGE AT ACCESSIBLE PARKING SPACES REQUIRED BY THE NC BUILDING CODE SECTION 1106.1SHALL COMPLY WITH THE REQUIREMENT:
- OF NORTH CAPOLINA GENERAL STATUTE 20-37.6 AND 136-30 AND THE NCDOT UNIFORM MANUAL ON TRAFFIC CONTROL DEVICES. A SEPARATE SIGN IS REQUIRED FOR EACH SPACE. SIGNS TO INDICATE THE MAXIMUM PENALTY MUST BE PROVIDED AT EACH ACCESSIBLE SPACE. 14. ACCESSIBLE PARKING SPACE, ACCESS AISLE STRIPING, AND INTERNATIONAL SYMBOL OF ACCESSIBILITY SHALL BE PAINTED BLUE (OR ANOTHER COLOR THAT CAN BE DISTINGUISHED FROM PAVEMENT).
- PASSENGER LOADING ZONE NOTES: 1. PASSENGER LOADING ZONES SHALL PROVIDE VEHICULAR PULL-UP SPACE NINETY-SIX (96) INCHES WIDE MINIMUM AND TWENTY (20) FEET LONG
- PASSENGER LOADING ZONES SHALL PROVIDE A CLEARLY MARKED ACCESS AISLE THAT IS SIXTY (60) INCHES WIDE MINIMUM AND EXTENDS THE FULL LENGTH OF THE VEHICLE PULL-UP SPACE THEY SERVE. 3. ACCESS AISLE SHALL ADJOIN AN ACCESSIBLE ROUTE AND NOT OVERLAP THE VEHICULAR WAY VEHICLE PULL-UP SPACES AND ACCESS AISLES SERVING THEM SHALL BE LEVEL WITH SURFACE SLOPES NOT EXCEEDING 2.0% IN ALL
- 5. FLOOR SURFACES OF VEHICLE PULL-UP SPACES AND ACCESS AISLES SERVING THEM SHALL BE STABLE, FIRM AND SLIP RESISTANT VEHICLE PULL-UP SPACES, ACCESS AISLES SERVING THEM AND A VEHICULAR ROUTE FROM AN ENTRANCE TO THE PASSENGER LOADING ZONE, AND FROM THE PASSENGER LOADING ZONE TO A VEHICULAR EXIT SERVING THEM, SHALL PROVIDE A VERTICAL CLEARANCE OF ONE HUNDRED FOURTEEN (114) INCHES MINIMUM.
- ACCESSIBLE ENTRANCES SHALL BE PROVIDED AS REQUIRED BY THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS) AND THE NORTH CAROLINA BUILDING CODE, AND APPLICABLE LOCAL LAWS & REGULATIONS. ENTRANCE DOORS, DOORWAYS AND GATES SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS) THE NC

CTIONS. ACCESS AISLES SHALL BE AT THE SAME LEVEL AS THE VEHICLE PULL-UP SPACE THEY SERVE. CHANGES IN LEVEL ARE NOT

GENERAL UTILITY NOTES

- ALL PROPOSED UTILITIES ARE PRIVATE UNLESS OTHERWISE NOTED.
- ALL STORM SEWERS SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF WILMINGTON REQUIREMENTS AS SPECIFIED ON THE DRAWINGS AND IN THE PROJECT SPECIFICATIONS.
- BEDDING FOR ALL STORM SEWER PIPE SHALL BE AS SPECIFIED ON THE DRAWINGS.
- ALL STORM SEWER PIPES SHOWN AS RCP ON THE PLANS SHALL BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM C-76,
- UNLESS INDICATED OTHERWISE ON PLANS.

- TOTAL SITE AREA = 34.993 AC
- DISTURBED AREA = SEE EROSION CONTROL PLANS
- ALLOCATED IMPERVIOUS AREA DRAINING TO STORMWATER SYSTEM = 1.084.491 SF (24.90 AC)
- RECEIVING STREAM = BARNARDS CREEK (C; Sw)

WETLAND NOTES:

NO WETLANDS EXISTS ON THIS SITE.

ROOF DRAIN NOTE:

IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO VERIFY THE ACTUAL LOCATION ANI

AVAILABILITY OF ALL EXISTING AND PROPOSED UTILITIES IN THE FIELD PRIOR TO GROUND BREAKING

PROPOSED BUILDING SHALL DIVERT ROOF DRAINAGE TO STORMWATER COLLECTION

EXISTING LITH ITIES AND STRUCTURES SHOWN BOTH UNDERGROUND AND ABOVE GROUND, ARE BASED ON A FIELD SURVEY AND THE BEST AVAILABLE RECORD DRAWINGS. THE CONTRACTOR SHALL FIELD VERIFY FIELD CONDITIONS PRIOR TO BEGINNING RELATED CONSTRUCTION. ANY DISCREPANCIES

SHALL BE REPORTED TO THE OWNER'S REPRESENTATIVE IMMEDIATELY.

= EXISTING WETLANDS = EXISTING PROPERTY LINE = EXISTING CONTOURS = IRON PIPE FOUND = IRON ROD FOUND

= TELEPHONE PEDESTAL = UNDERGROUND TELEPHONE

= PROPOSED INLET

= CONCRETE MONUMENT

= PROPOSED STORM SEWER

= PROPOSED STORM DRAIN MANHOLE

= PROPOSED STORM DRAIN MANHOLE

= PROPOSED DRAINAGE AREAS

= TRANSFORMER PEDESTAL (PAD)

- = PROPOSED CURB INLET = PROPOSED DROP INLET
- PG=49.90 = PROPOSED SPOT ELEVATIONS = PROPOSED CONTOUR -----35-----
- = PROPOSED SILT FENCE = PROPOSED LIMITS OF DISTURBANCE = PROPOSED TREE PROTECTION FENCING
- = PROPOSED INLET PROTECTION = PROPOSED TRAFFIC ARROW = PROPOSED WOOD SHADOWBOX FENCE

C.O.

- = PROPOSED SIGN = PROPOSED SANITARY SEWER SERVICE = PROPOSED SANITARY SEWER CLEANOUT
- = EX. ASPHALT AREA TO BE MILLED 1-INCH AND OVERLAYED = PROPOSED ASPHALT PAVEMENT SECTION

= PROPOSED CONCRETE CURB & GUTTER

IN THE EVENT THAT THE GOVERNING AGENCIES TIMEFRAME FOR STABILIZATION VARY. CONTRACTOR SHALL MEET THE MORE STRINGENT REQUIREMENT. NPDES WATER QUALITY STABILIZATION TIME FRAMES SITE AREA DESCRIPTION STABILIZATION TIMEFRAME EXCEPTIONS PERIMETER DIKES, SWALES, DITCHES AND SLOPES 7 DAYS NONE HIGH QUALITY WATER (HQW) ZONES 7 DAYS NONE SLOPES STEEPER THAN 3:1 F SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED

7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH

NONE, EXCEPT FOR PERIMETERS AND HQW ZONES

IEW HANOVER COUNTY LAND QUALITY STABILIZATION TIME FRAME NOTE:
DENUDED AREAS MUST BE STABILIZED WITH FIFTEEN (15) WORKING DAYS OF CEASE OF ANY PHASE OF ACTIVITY. ALL SLOPES MUST BE STABILIZED WITHIN TWENTY-ONE (21) CALENDAR DAYS OF CEASE OF ANY PHASE OF ACTIVITY. THIS INCLUDES SLOPES, SWALES,

14 DAYS

14 DAYS

NO PAINT OR LIQUID WASTES IN STREAM OR STORM DRAINS.

ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1

DEDICATED AREAS FOR DEMOLITION, CONSTRUCTION AND OTHER WASTES MUST BE LOCATED 50' FROM STORM DRAINS AND STREAMS UNLESS NO REASONABLE ALTERNATIVES AVAILABLE. 3. EARTHEN-MATERIALS STOCKPILES MUST BE LOCATED 50' FROM STORM DRAINS AND STREAMS UNLESS NO REASONABLE

INSPECTION REPORTS MUST BE AVAILABLE ON-SITE DURING BUSINESS HOURS UNLESS A SITE-SPECIFIC EXEMPTION IS

THIS PAGE CAN BE APPROVED BY THE COUNTY PURSUANT TO NPDES GENERAL STORMWATER PERMIT NCG010000 ONLY

THIS PAGE OF THE APPROVED PLANS IS ENFORCEABLE EXCLUSIVELY PURSUANT TO NPDES GENERAL STORMWATER PERMIT

4. THE COUNTY IS NOT AUTHORIZED TO ENFORCE THE NPDES PORTION OF THIS PAGE OF THE PLANS AND THEY ARE NOT A PART

4. CONCRETE MATERIALS MUST BE CONTROLLED TO AVOID CONTACT WITH SURFACE WATERS, WETLANDS, OR BUFFERS.

- SAME RAIN GAUGE & INSPECTIONS AFTER 0.5" RAIN EVENT. INSPECTIONS ARE ONLY REQUIRED DURING "NORMAL BUSINESS HOURS".
- RECORDS MUST BE KEPT FOR 3 YEARS AND AVAILABLE UPON REQUEST. 6. ELECTRONICALLY AVAILABLE RECORDS MAY BE SUBSTITUTED UNDER CERTAIN CONDITIONS.
- OUTLET STRUCTURES MUST WITHDRAW FROM BASIN SURFACE UNLESS DRAINAGE AREA IS LESS THAN 1 ACRE. USE ONLY DWQ-APPROVED FLOCULANTS. NPDES - SPECIFIC PLAN SHEETS NOTES: THIS PAGE IS SUBMITTED TO COMPLY WITH NPDES GENERAL STORMWATER PERMIT NCG010000.

OF THE APPROVED PLANS FOR THE PURPOSES OF ENFORCEMENT ACTION UNDER THE COUNTY CODE.

SLOPES 3:1 OR FLATTER

CHANNELS AND STOCKPILES.

ALTERNATIVES AVAILABLE

NPDES SEDIMENT BASINS:

SRB-6-1215 CONDITIONS:

- THE DEVELOPMENT OF THE SUBJECT PROPERTY SHALL BE IN ACCORDANCE WITH THE PRELIMINARY PLAN AS APPROVED BY APPROVAL OF THIS PRELIMINARY PLAN DOES NOT CONSTITUTE TECHNICAL APPROVAL OF THE SITE PLAN. FINAL APPROVAL BY
- THE TECHNICAL REVIEW COMMITTEE AND THE ISSUANCE OF ALL REQUIRED PERMITS MUST OCCUR PRIOR TO RELEASE OF THE PROJECT FOR CONSTRUCTION.
- UTILITIES SHALL BE LOCATED UNDER THE ASPHALT TO AVOID LANDSCAPING CONFLICTS IN ACCORDANCE WITH THE CITY TECHNICAL STANDARDS, MULTI-USE PATH SHALL BE INSTALLED BY THE DEVELOPER ALONG
- THE ENTIRE FRONTAGES OF S. 17TH STREET. IN ACCORDANCE WITH THE CITY TECHNICAL STANDARDS, SIDEWALK SHALL BE INSTALLED BY THE DEVELOPER ALONG THE
- ENTIRE FRONTAGES OF INDEPENDENCE BOULEVARD. TREES DESIGNATED TO BE PRESERVED MUST BE PROPERLY PROTECTED DURING CONSTRUCTION AND TREE PROTECTION
- FENCING SHALL BE NOTED ON THE CONSTRUCTION PLANS ALL CITY, STATE AND FEDERAL REGULATIONS SHALL BE ME
- ALL TECHNICAL REVIEW COMMITTEE COMMENTS SHALL BE FOLLOWED. AN ACCEPTABLE PRELIMINARY PLAN WHICH ADEQUATELY ADDRESSES ALL TRC/SRB COMMENTS SHALL BE SUBMITTED FOR THE SUBDIVISION COORDINATOR'S SIGNATURE WITHIN THIRTY (30) CALENDAR DAYS OF SRB APPROVAL.
- ALL EXISTING AND PROPOSED PUBLIC AND NON-MUNICIPAL EASEMENTS SHALL BE ADDED TO THE PRELIMINARY PLAN AS TENANTS / ON-SITE OWNER-OCCUPANTS AND BUSINESSES OPERATING WITHIN THE SUBDIVISION SHALL AGREE THAT VEHICLE

LOADING SHALL OCCUR AT OFF-PEAK HOURS.

- A WAIVER FROM THE MINIMUM REQUIRED OFFSETS BETWEEN CENTERLINES OF INTERSECTIONS
- A WAIVER FROM THE MAXIMUM LENGTH OF A PRIVATE DRIVEWAY A WAIVER FROM THE MINIMUM CENTERLINE CURVE RADII
- A WAIVER FROM PROVIDING SIDEWALK ON BOTH SIDES OF THE STREET
- A WAIVER TO ALLOW THE USE OF SLOPED OR FLUSH CURB IN SELECT LOCATIONS A WAIVER FROM THE MINIMUM PAVEMENT WIDTH
- A WAIVER TO ALLOW ANGLED PARKING ON A PRIVATE STREET A WAIVER TO ALLOW THE INTEGRATION OF LOADING SPACES WITHIN THE PROPOSED PARKING CONFIGURATION. THIS WAIVER WAS CONDITIONALLY APPROVED TO REQUIRE THE ENFORCEMENT OF OFF-PEAK LOADING WITHIN THE SUBDIVISION TO BE

RECORDED IN THE RESTRICTIVE COVENANTS OF THE PROPERTY OWNERS ASSOCIATION.

- SRB-6-1215-M518 CONDITIONS: THE USE AND DEVELOPMENT OF THE SUBJECT PROPERTY SHALL COMPLY WITH ALL REGULATIONS AND REQUIREMENTS IMPOSED BY THE LAND DEVELOPMENT CODE. THE CITY OF WILMINGTON TECHNICAL STANDARDS AND SPECIFICATIONS MANUAL AND ANY OTHER APPLICABLE FEDERAL, STATE OR LOCAL LAW, ORDINANCE OR REGULATION, AS WELL AS ANY CONDITION STATED BELOW. IN THE EVENT OF A CONFLICT. THE MORE STRINGENT REQUIREMENTS OR HIGHER STANDARD SHALL APPLY APPROVAL OF THIS PRELIMINARY PLAN DOES NOT CONSTITUTE TECHNICAL APPROVAL OF THE SITE PLAN. FINAL APPROVAL BY
- THE TECHNICAL REVIEW COMMITTEE AND THE ISSUANCE OF ALL REQUIRED PERMITS MUST OCCUR PRIOR TO RELEASE OF THE IF, FOR ANY REASON, ANY CONDITION FOR APPROVAL IS FOUND TO BE ILLEGAL OR INVALID OR IF THE APPLICANT SHOULD FAIL TO ACCEPT ANY CONDITION FOLLOWING APPROVAL, THE APPROVAL OF THE PRELIMINARY PLAN SHALL BE NULL AND VOID.
- THE USE AND DEVELOPMENT OF THE SUBJECT PROPERTY SHALL BE IN ACCORDANCE WITH THE PRELIMINARY PLAN AS SUBMITTED AND APPROVED. UTILITIES SHALL BE LOCATED UNDER THE ASPHALT TO AVOID LANDSCAPING CONFLICTS. AN 8 TO 10-FOOT WIDE PAVED MULTI-USE PATH SHALL BE PROVIDED ALONG THE FRONTAGE OF S. 17TH STREET AND A SIDEWALK IS REQUIRED ALONG THE ENTIRE INDEPENDENCE BOULEVARD FRONTAGE OF THE SUBJECT PROPERTY
- REQUIREMENTS OF THE APPROVED TRAFFIC IMPACT ANALYSIS (TIA) MUST BE COMPLETED PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY. ALL CITY, STATE AND FEDERAL REGULATIONS SHALL BE MET.

THE SIGN LOCATED ON THE CORNER OF GALLERY PARK BOULEVARD AND S. 17TH STREET SHALL COMPLY AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) STANDARDS FOR SIGHT DISTANCE. SRB-6-1215-M518 WAIVERS:

- A WAIVER TO ALLOW STREET TYPE ENTRANCES FOR PRIVATE DRIVES. PRIVATE DRIVES ARE EXISTING STREET ENTRANCES AND PEDESTRIAN AND VEHICULAR CONFLICTS HAVE BEEN ADDRESSEI A WAIVER FROM THE MAXIMUM DRIVEWAY WIDTH. DRIVEWAYS ARE EXISTING AND HAVE BEEN SET UP AS ROADWAYS WITH MEDIANS. THERE IS CLEAR DIRECTION PROVIDED FOR DRIVES.
- A WAIVER FROM THE MINIMUM DRIVEWAY OFFSET FROM THE PROPERTY LINE. THE MODIFICATION WOULD RESULT IN AN EXISTING DRIVE/ACCESS BEING SPLIT BY A PROPERTY LINE. ALL PREVIOUSLY APPROVED WAIVERS WILL REMAIN ON PLACE, AS APPLICABLE.

EXHIBIT (SEE CD-7-213 ATTACHMENT 3) DATED RECEIVED JULY 31, 2013.

CD-7-213 CONDITIONS:

SHALL APPLY.

THE PROPERTY SHALL BE SUBJECT TO ALL OF THE SPECIFIC REQUIREMENTS STATED IN THE LAND DEVELOPMENT CODE FOR THE PROPOSED USE AS WELL AS ANY ADDITIONAL CONDITIONS STATED BELOW. IF, FOR ANY REASON, ANY CONDITION FOR APPROVAL IS FOUND TO BE ILLEGAL OR INVALID OR IF THE APPLICANT SHOULD FAIL TO ACCEPT ANY CONDITION FOLLOWING APPROVAL, THE APPROVAL FOR THE SITE PLAN FOR THE DISTRICT SHALL BE NULL AND VOID AND OF NO EFFECT AND PROCEEDINGS SHALL BE INSTITUTED TO REZONE THE PROPERTY TO ITS PREVIOUS ZONING CLASSIFICATION

THE USE AND DEVELOPMENT OF THE SUBJECT PROPERTY SHALL BE IN ACCORDANCE WITH THE MASTER PLAN / REZONING

THE MASTER PLAN (REZONING EXHIBIT) SHALL ESTABLISH NODES AND INTERVENING NON-COMMERCIAL USES. APPLICANT

CONCEPTUAL PLAN FOR THE DEVELOPMENT. THE APPLICANT SHALL UTILIZE MONUMENT SIGNAGE AND WORK WITH CITY STAFF

THE RECREATION AND OPEN SPACE REQUIREMENTS AS DICTATED BY THE LAND DEVELOPMENT CODE FOR MULTI-FAMILY LISES

- SHALL WORK WITH STAFF TO INCLUDE THE DESIGNATION OF PUBLIC SPACES AT EACH MAJOR NODE AND ENSURE THAT ADEQUATE PUBLIC SPACE IS PROVIDED AT EACH MAJOR NODE HOMEOWNERS / PROPERTY OWNERS (HOA / POA) DOCUMENTS SHALL BE RECORDED FOR ALL PARCELS AND SHALL REFLECT DESIGN AND PLANNING CONCERNS AS OUTLINED IN THE LAND DEVELOPMENT CODE AND THE CD-7-213 CASE SUMMARY. ARCHITECTURAL CONSISTENCY AND CONTROLS REGARDING THEME AND PROJECT IDENTITY SHALL BE PROVIDED PRIOR TO SITE PLAN APPROVALS, THE APPLICANT SHALL SUBMIT A MASTER SIGNAGE PLAN THAT COMPLIMENTS THE MASTER
- IN ORDER TO ACHIEVE THE STATED FLUP OBJECTIVES FOR SIGNAGE IN BARCLAY WEST. THE USE AND DEVELOPMENT OF THE SUBJECT PROPERTY SHALL COMPLY WITH ALL OTHER SUPPLEMENTAL REGULATIONS AND REQUIREMENTS IMPOSED BY THE LAND DEVELOPMENT CODE OR ANY OTHER APPLICABLE FEDERAL, STATE OR LOCAL LAW, ORDINANCE OR REGULATION. IN THE EVENT OF A CONFLICT, THE MORE STRINGENT REQUIREMENT OR HIGHER STANDARD
- ALL DEVELOPMENT WITHIN BARCLAY WEST SHALL COMPLY WITH LAND DEVELOPMENT CODE REQUIREMENTS FOR SITE PLAN REVIEW AND APPROVAL. SUBDIVISION REVIEW AND APPROVAL SHALL BE REQUIRED AS DICTATED BY CITY CODE. THE MAXIMUM ALLOWABLE NUMBER OF RESIDENTIAL UNITS IN AREA OF BARCLAY WEST ZONED MF-M (CD) SHALL BE 1,259 UNITS. THE REMAINDER OF THE 1,375 RESIDENTIAL UNITS MAY BE LOCATED IN AREAS OF BARCLAY WEST ZONED RB (CD) AND O&I-1
- SHALL BE MET INDEPENDENTLY AND DISTINCT FROM NON MULTI-FAMILY ZONED AREAS OF BARCLAY WEST APPROVAL OF THE MASTER CONCEPT PLAN / REZONING EXHIBIT SHALL NOT CONSTITUTE APPROVAL FOR ANY SPECIFIC PHASE OF DEVELOPMENT. DEVELOPMENT PLANS SHALL BE SUBMITTED, REVIEWED, AND APPROVED BY THE TRC. ALL TRC REQUIREMENTS SHALL BE MET ALL CITY STATE AND FEDERAL REGULATIONS SHALL BE MET THE APPLICANT SHALL COORDINATE INTERNAL TRAFFIC CIRCULATION WITH PROVISIONS OF PUBLIC OPEN SPACE AMENITIES. THE CONNECTOR ROADS AS SHOWN ON THE MASTER PLAN (REZONING EXHIBIT) SHALL PROVIDE INTEGRATED VEHICULAR
- TRAFFIC PATTERNS WITHIN THE PROJECT AND ALLEVIATE INCREASED TRAFFIC ON NEARBY MAJOR THOROUGHFARES. A VALID TRAFFIC IMPACT ANALYSIS ("TIA") SHALL BE SUBMITTED TO THE WILMINGTON METROPOLITAN PLANNING ORGANIZATION ("WMPO") FOR REVIEW BY THE WMPO. THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION AND THE CITY OF WILMINGTON TRAFFIC ENGINEER LIPON CONCLUSION OF THE REVIEW IMPROVEMENTS REQUIRED BY THE DEVELOPER (TO MITIGATE TRAFFIC IMPACTS OF THE PROPOSED DEVELOPMENT) WILL BE OUTLINED IN A TIA APPROVAL LETTER. THE

IMPROVEMENTS SHALL BE MADE BY THE OWNERS AND AT THEIR EXPENSE.

DEVELOPER SHALL SUBMIT A SEPARATE PHASING STUDY FOR EACH PHASE OF DEVELOPMENT TO DETERMINE WHICH IMPROVEMENTS IDENTIFIED IN THE TIA APPROVAL LETTER SHALL BE CONSTRUCTED WITH THE DEVELOPMENT PHASE. ALL DEVELOPER REQUIRED IMPROVEMENTS FOR EACH PHASE SHALL BE CONSTRUCTED BY THE DEVELOPER PRIOR TO THE ISSUANCE OF CERTIFICATES OF OCCUPANCY IN THE RESPECTIVE PHASES OF DEVELOPMENT. ANY NECESSARY ACQUISITION AND / OR DEDICATION OF LAND FOR RIGHTS-OF-WAY FOR DEVELOPER REQUIRED

ACCESS FOR TRAFFIC TO DEVELOPMENT PARCELS. THE PURPOSE OF THIS NETWORK OF ROADS SHALL BE TO MAINTAIN

INTERIOR RIGHTS-OF-WAY WITHIN BARCLAY WEST. IT SHALL BE THE RESPONSIBILITY OF THE DEVELOPER TO INSTALL THESE TRAFFIC CALMING DEVICES. THE APPLICANT SHALL WORK WITH THE CAPE FEAR TRANSIT AUTHORITY TO ENSURE THAT BARCLAY WEST OFFERS ADEQUATE PUBLIC TRANSIT SERVICE. LINEAR STRIP RETAIL AND SERVICE DEVELOPMENT SHALL BE PROHIBITED

THE APPLICANT SHALL WORK WITH STAFF TO INSTALL ADEQUATE TRAFFIC-CALMING MEASURES WITHIN THE PROPOSED PUBLIC

THE APPLICANT SHALL WORK WITH STAFF AND DEMONSTRATE INCREASED PRESERVATION OF TREES IN EXCESS OF MINIMUM

THE APPLICANT AGREES TO PROVIDE ENVIRONMENTAL RESOURCE DELINEATIONS SO THAT EXISTING NATURAL FEATURES ARE

IDENTIFIED ON THE SITE. APPLICANT SHALL WORK WITH STAFF TO PRESERVE AND ENHANCE THE EXISTING NATURAL FEATURES

THE APPLICANT SHALL WORK WITH CITY PLAN REVIEW ENGINEERING STAFF SO AS TO DETERMINE BEST STORMWATER MANAGEMENT PRACTICES THAT SERVE THE SUBJECT PROPERTY. STORMWATER BMP'S MAY BE USED TO DELINEATE FUTURE LAND BAYS SO LONG AS THE BMP IS DESIGNED AS AN AESTHETIC SITE AMENITY. THE APPLICANT SHALL WORK WITH STAFF TO DEMONSTRATE THAT AMPLE OPEN SPACE IS BEING PROVIDED IN THE BARCLAY WEST DEVELOPMENT RESULTING FROM THE USE OF MULTIPLE STORY DEVELOPMENT AND PARKING DECKS.

THE APPLICANT SHALL INTEGRATE RESIDENTIAL, OFFICE, RETAIL, INSTITUTIONAL AND ENTERTAINMENT USES.

AND INTEGRATE THEM INTO THE OPEN SPACE BEING PROVIDED IN THE DEVELOPMENT. AS SHOWN ON THE MASTER PLAN / REZONING EXHIBIT (CD-7-213 ATTACHMENT 3 DATED RECEIVED JULY 31, 2013), THE ACCESS POINT DESIGNATED TO CONNECT HANOVER HEIGHTS WITH BARCLAY WEST AT NEWKIRK DRIVE SHALL BE LIMITED TO USE BY

CONSTRUCTION.

shall be required from the

City prior to occupancy

PEDESTRIANS, BICYCLES AND EMERGENCY VEHICLES. FIRE AND LIFE SAFETY NOTES:

CONTRACTOR SHALL MAINTAIN AN ALL-WEATHER ACCESS FOR EMERGENCY VEHICLES AT ALL TIMES DURING

IN ADDITION TO THE STANDARD COMMENTS, ADDITIONAL FIRE PROTECTION AND ACCESSIBILITY REQUIREMENTS MAY

ALL ISOLATION VALVES WITHIN THE "HOT BOX" SHALL BE ELECTRICALLY SUPERVISED. PLEASE WORK CLOSELY WITH

CONTRACTOR SHALL SUBMIT A RADIO SIGNAL STRENGTH STUDY THAT DEMONSTRATES THAT EXISTING EMERGENCY

NEW HYDRANTS MUST BE BROUGHT INTO SERVICE PRIOR TO COMBUSTIBLE MATERIALS DELIVERED TO THE JOB SITE. HYDRANT MUST BE WITHIN 150' OF THE FDC (MEASURED AS THE TRUCK DRIVES FOR PRACTICAL USE). LANDSCAPING OR PARKING CANNOT BLOCK OR IMPEDE THE FDC OR FIRE HYDRANTS. A 3-FOOT CLEAR SPACE SHALL BE MAINTAINED AROUND THE CIRCUMFERENCE OF THE HYDRANT AND FDC. PRIVATE UNDERGROUND FIRE LINES REQUIRE A SEPARATE UNDERGROUND FIRE LINE PERMIT FROM THE

YOUR FIRE SPRINKLER AND ALARM INSTALLER IN REGARDS TO RUNNING WIRE FOR TAMPER SWITCH.

RESPONDER RADIO SIGNAL LEVELS MEET THE REQUIREMENTS OF SECTION 510 OF THE 2018 NC FIRE CODE.

WILMINGTON FIRE AND LIFE SAFETY DIVISION 910-343-0696.

BE REQUIRED DUE TO ANY SPECIAL CIRCUMSTANCES CONCERNING THE PROJECT.

FDC MUST BE WITHIN 40' OF FIRE APPARATUS PLACEMENT

Approved Construction Plan For each open utility cut of City streets, a \$325 permit

and/or project acceptance Public Services • Engineering Division APPROVED STORMWATER MANAGEMENT PLAN **Know what's below.**

fotel

edacted on electroni

PEI JOB#: 18485.PE

Professional Seal

copy per City of Wilmington Policy

GROUND STABILIZATION AND MATERIALS HANDLING PRACTICES FOR COMPLIANCE WITH THE NCG01 CONSTRUCTION GENERAL PERMIT mplementing the details and specifications on this plan sheet will result in the construction activity being considered compliant with the Ground Stabilization and Materials Handling sections of the NCG01 Construction General Permit (Sections E and F, respectively). The ermittee shall comply with the Erosion and Sediment Control plan approved by the elegated authority having jurisdiction. All details and specifications shown on this sheet may not apply depending on site conditions and the delegated authority having jurisdiction. SECTION E: GROUND STABILIZATION Required Ground Stabilization Timeframes Stabilize within this many calendar Timeframe variations days after ceasing and disturbance (a) Perimeter dikes, swales, ditches, and perimeter slopes (b) High Quality Water (HQW) Zones If slopes are 10' or less in length and are Slopes steeper than

flatter than 4:1 there is zero slope **Note:** After the permanent cessation of construction activities, any areas with temporary ground stabilization shall be converted to permanent ground stabilization as soon as practicable but in no case longer than 90 calendar days after the last land disturbing activity. Temporary ground stabilization shall be maintained in a manner to render the

surface stable against accelerated erosion until permanent ground stabilization is achieved.

GROUND STABILIZATION SPECIFICATION

(d) Slopes 3:1 to 4:1

(e) Areas with slopes

Plastic sheeting

Stabilize the ground sufficiently so that rain will not dislodge the soil. Use one of the techniques in the table below:

- Temporary grass seed covered with straw or | Permanent grass seed covered with straw or other mulches and tackifiers other mulches and tackifiers Hydroseeding • Rolled erosion control products with or
- without temporary grass seed Appropriately applied straw or other mulch
 - Geotextile fabrics such as permanent soil reinforcement matting Hydroseeding Shrubs or other permanent plantings covered with mulch Uniform and evenly distributed ground cover

sufficient to restrain erosion

retaining walls

not steeper than 2:1, 14 days are

7 days for slopes greater than 50' in

length and with slopes steeper than 4:1

7 days for perimeter dikes, swales,

ditches, perimeter slopes and HQW

-10 days for Falls Lake Watershed

days for perimeter dikes, swales,

Permanent Stabilization

• Structural methods such as concrete, asphalt or

Rolled erosion control products with grass seed

ditches, perimeter slopes and HQW Zones

-10 days for Falls Lake Watershed unless

POLYACRYLAMIDES (PAMS) AND FLOCCULANTS

- Select flocculants that are appropriate for the soils being exposed during construction, selecting from the NC DWR List of Approved PAMS/Flocculants.
- Apply flocculants at or before the inlets to Erosion and Sediment Control Measures. Apply flocculants at the concentrations specified in the NC DWR List of Approved *PAMS/Flocculants* and in accordance with the manufacturer's instructions.
- Provide ponding area for containment of treated Stormwater before discharging Store flocculants in leak-proof containers that are kept under storm-resistant cover

or surrounded by secondary containment structures.

EQUIPMENT AND VEHICLE MAINTENANCE

has been corrected.

Provide drip pans under any stored equipment.

- Maintain vehicles and equipment to prevent discharge of fluids.
- Identify leaks and repair as soon as feasible, or remove leaking equipment from the
- Collect all spent fluids, store in separate containers and properly dispose as hazardous waste (recycle when possible).
- Bring used fuels, lubricants, coolants, hydraulic fluids and other petroleum products to a recycling or disposal center that handles these materials.

Remove leaking vehicles and construction equipment from service until the problem

LITTER, BUILDING MATERIAL AND LAND CLEARING WASTE

- Never bury or burn waste. Place litter and debris in approved waste containers. Provide a sufficient number and size of waste containers (e.g dumpster, trash receptacle) on site to contain construction and domestic wastes.
- Locate waste containers at least 50 feet away from storm drain inlets and surface waters unless no other alternatives are reasonably available.
- Locate waste containers on areas that do not receive substantial amounts of runoff from upland areas and does not drain directly to a storm drain, stream or wetland.
- Cover waste containers at the end of each workday and before storm events or provide secondary containment. Repair or replace damaged waste containers.
- Anchor all lightweight items in waste containers during times of high winds. Empty waste containers as needed to prevent overflow. Clean up immediately if

9. On business days, clean up and dispose of waste in designated waste containers.

containers overflow. 8. Dispose waste off-site at an approved disposal facility.

PAINT AND OTHER LIQUID WASTE

- Do not dump paint and other liquid waste into storm drains, streams or wetlands. Locate paint washouts at least 50 feet away from storm drain inlets and surface
- waters unless no other alternatives are reasonably available. Contain liquid wastes in a controlled area.
- Containment must be labeled, sized and placed appropriately for the needs of site.
- 5. Prevent the discharge of soaps, solvents, detergents and other liquid wastes from

PORTABLE TOILETS

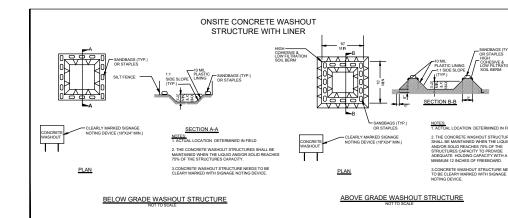
construction sites.

- Install portable toilets on level ground, at least 50 feet away from storm drains, streams or wetlands unless there is no alternative reasonably available. If 50 foot offset is not attainable, provide relocation of portable toilet behind silt fence or place on a gravel pad and surround with sand bags.
- Provide staking or anchoring of portable toilets during periods of high winds or in high foot traffic areas.
- Monitor portable toilets for leaking and properly dispose of any leaked material. Utilize a licensed sanitary waste hauler to remove leaking portable toilets and replace with properly operating unit.

EARTHEN STOCKPILE MANAGEMENT

- Show stockpile locations on plans. Locate earthen-material stockpile areas at least 50 feet away from storm drain inlets, sediment basins, perimeter sediment controls and surface waters unless it can be shown no other alternatives are reasonably
- Protect stockpile with silt fence installed along toe of slope with a minimum offset of five feet from the toe of stockpile.
- Provide stable stone access point when feasible.
- with the approved plan and any additional requirements. Soil stabilization is defined as vegetative, physical or chemical coverage techniques that will restrain accelerated erosion on disturbed soils for temporary or permanent control needs.

Stabilize stockpile within the timeframes provided on this sheet and in accordance



CONCRETE WASHOUTS

- 1. Do not discharge concrete or cement slurry from the site.
- 2. Dispose of, or recycle settled, hardened concrete residue in accordance with local and state solid waste regulations and at an approved facility.
- . Manage washout from mortar mixers in accordance with the above item and in addition place the mixer and associated materials on impervious barrier and within lot perimeter silt fence.
- Install temporary concrete washouts per local requirements, where applicable. If an alternate method or product is to be used, contact your approval authority for review and approval. If local standard details are not available, use one of the two types of temporary concrete washouts provided on this detail.
- . Do not use concrete washouts for dewatering or storing defective curb or sidewalk sections. Stormwater accumulated within the washout may not be pumped into or discharged to the storm drain system or receiving surface waters. Liquid waste must be pumped out and removed from project.
- can be shown that no other alternatives are reasonably available. At a minimum, install protection of storm drain inlet(s) closest to the washout which could receive spills or overflow.
- Locate washouts in an easily accessible area, on level ground and install a stone entrance pad in front of the washout. Additional controls may be required by the

Locate washouts at least 50 feet from storm drain inlets and surface waters unless it

- approving authority. . Install at least one sign directing concrete trucks to the washout within the project
- limits. Post signage on the washout itself to identify this location. Remove leavings from the washout when at approximately 75% capacity to limit overflow events. Replace the tarp, sand bags or other temporary structural components when no longer functional. When utilizing alternative or proprietary
- products, follow manufacturer's instructions. 10. At the completion of the concrete work, remove remaining leavings and dispose of in an approved disposal facility. Fill pit, if applicable, and stabilize any disturbance
- caused by removal of washout.

HERBICIDES, PESTICIDES AND RODENTICIDES

- Store and apply herbicides, pesticides and rodenticides in accordance with label
- Store herbicides, pesticides and rodenticides in their original containers with the label, which lists directions for use, ingredients and first aid steps in case of accidental poisoning.
- Do not store herbicides, pesticides and rodenticides in areas where flooding is possible or where they may spill or leak into wells, stormwater drains, ground water or surface water. If a spill occurs, clean area immediately.
- Do not stockpile these materials onsite.

HAZARDOUS AND TOXIC WASTE

- Create designated hazardous waste collection areas on-site.
- Place hazardous waste containers under cover or in secondary containment. Do not store hazardous chemicals, drums or bagged materials directly on the ground.

NCG01 GROUND STABILIZATION AND MATERIALS HANDLING

EFFECTIVE: 04/01/19

SELF-INSPECTION, RECORDKEEPING AND REPORTING

Self-inspections are required during normal business hours in accordance with the table below. When adverse weather or site conditions would cause the safety of the inspection personnel to be in jeopardy, the inspection may be delayed until the next business day on which it is safe to perform the inspection. In addition, when a storm event of equal to or greater than 1.0 inch occurs outside of normal business hours, the self-inspection shall be performed upon the commencement of the next business day. Any time when inspections were delayed shall be noted in the Inspection Record.

| Inspect | (during normal business hours) | Inspection records must include: | |
|--|--|---|--|
| (1) Rain gauge maintained in good working order | Daily | Daily rainfall amounts. If no daily rain gauge observations are made during weekend or holiday periods, and no individual-day rainfall information is available, record the cumulative rain measurement for those unattended days (and this will determine if a site inspection is needed). Days on which no rainfall occurred shall be recorded as "zero." The permittee may use another rain-monitoring device approved by the Division. | |
| (2) E&SC Measures | At least once per 7 calendar days and within 24 hours of a rain event ≥ 1.0 inch in 24 hours | Identification of the measures inspected, Date and time of the inspection, Name of the person performing the inspection, Indication of whether the measures were operating properly, Description of maintenance needs for the measure, Description, evidence, and date of corrective actions taken. | |
| (3) Stormwater discharge outfalls (SDOs) | At least once per 7 calendar days and within 24 hours of a rain event ≥ 1.0 inch in 24 hours | Identification of the discharge outfalls inspected, Date and time of the inspection, Name of the person performing the inspection, Evidence of indicators of stormwater pollution such as oil sheen, floating or suspended solids or discoloration, Indication of visible sediment leaving the site, Description, evidence, and date of corrective actions taken. | |
| (4) Perimeter of site | At least once per 7 calendar days and within 24 hours of a rain event ≥ 1.0 inch in 24 hours | If visible sedimentation is found outside site limits, then a record of the following shall be made: Actions taken to clean up or stabilize the sediment that has left the site limits, Description, evidence, and date of corrective actions taken, and An explanation as to the actions taken to control future releases. | |
| (5) Streams or wetlands onsite or offsite (where accessible) | At least once per 7 calendar days and within 24 hours of a rain event ≥ 1.0 inch in 24 hours | If the stream or wetland has increased visible sedimentation or a stream has visible increased turbidity from the construction activity, then a record of the following shall be made: 1. Description, evidence and date of corrective actions taken, and 2. Records of the required reports to the appropriate Division Regional Office per Part III, Section C, Item (2)(a) of this permit of this permit. | |
| (6) Ground stabilization measures | After each phase of grading | 1. The phase of grading (installation of perimeter E&SC measures, clearing and grubbing, installation of storm drainage facilities, completion of all land-disturbing activity, construction or redevelopment, permanent ground cover). 2. Documentation that the required ground stabilization measures have been provided within the required timeframe or an assurance that they will be provided as soon as possible. | |

NOTE: The rain inspection resets the required 7 calendar day inspection requirement.

SELF-INSPECTION, RECORDKEEPING AND REPORTING

SECTION B: RECORDKEEPING . E&SC Plan Documentation

Item to Document

The approved E&SC plan as well as any approved deviation shall be kept on the site. The approved E&SC plan must be kept up-to-date throughout the coverage under this permit. The following items pertaining to the E&SC plan shall be documented in the manner described:

Documentation Requirements

| (a) Each E&SC Measure has been installed and does not significantly deviate from the locations, dimensions and relative elevations shown on the approved E&SC Plan. | Initial and date each E&SC Measure on a copy of the approved E&SC Plan or complete, date and sign an inspection report that lists each E&SC Measure shown on the approved E&SC Plan. This documentation is required upon the initial installation of the E&SC Measures or if the E&SC Measures are modified after initial installation. |
|---|---|
| (b) A phase of grading has been completed. | Initial and date a copy of the approved E&SC Plan or complete, date and sign an inspection report to indicate completion of the construction phase. |
| (c) Ground cover is located and installed n accordance with the approved E&SC Plan. | Initial and date a copy of the approved E&SC Plan or complete, date and sign an inspection report to indicate compliance with approved ground cover specifications. |
| (d) The maintenance and repair requirements for all E&SC Measures have been performed. | Complete, date and sign an inspection report. |
| (e) Corrective actions have been taken to E&SC Measures. | Initial and date a copy of the approved E&SC Plan or complete, date and sign an inspection report to indicate the completion of the corrective action. |

2. Additional Documentation

In addition to the E&SC Plan documents above, the following items shall be kept on the and available for agency inspectors at all times during normal business hours, unless the

Division provides a site-specific exemption based on unique site conditions that make this

requirement not practical: (a) This general permit as well as the certificate of coverage, after it is received. (b) Records of inspections made during the previous 30 days. The permittee shall record the required observations on the Inspection Record Form provided by the Division or

electronically-available records in lieu of the required paper copies will be allowed if

All data used to complete the Notice of Intent and older inspection records shall be maintained for a period of three years after project completion and made available upon request. [40 CFR 122.41]

a similar inspection form that includes all the required elements. Use of

shown to provide equal access and utility as the hard-copy records.

SELF-INSPECTION, RECORDKEEPING AND REPORTING

SECTION C: REPORTING

1. Occurrences that must be reported Permittees shall report the following occurrences:

(a) Visible sediment deposition in a stream or wetland.

(b) Oil spills if:

stream or wetland

- They are 25 gallons or more,
- They are less than 25 gallons but cannot be cleaned up within 24 hours,
- They cause sheen on surface waters (regardless of volume), or • They are within 100 feet of surface waters (regardless of volume).

(a) Visible sediment • Within 24 hours, an oral or electronic notification.

-) Releases of hazardous substances in excess of reportable quantities under Section 311 of the Clean Water Act (Ref: 40 CFR 110.3 and 40 CFR 117.3) or Section 102 of CERCLA (Ref: 40 CFR 302.4) or G.S. 143-215.85.
- (b) Anticipated bypasses and unanticipated bypasses.
- c) Noncompliance with the conditions of this permit that may endanger health or the environment.

. Reporting Timeframes and Other Requirements

After a permittee becomes aware of an occurrence that must be reported, he shall contact the appropriate Division regional office within the timeframes and in accordance with the other requirements listed below. Occurrences outside normal business hours may also be reported to the Division's Emergency Response personnel at (800) 662-7956, (800) 858-0368 or (919) 733-3300.

sediment and actions taken to address the cause of the deposition.

Division staff may waive the requirement for a written report on a

deposition in a

• Within 7 calendar days, a report that contains a description of the

| | If the stream is named on the NC 303(d) list as impaired for sediment-related causes, the permittee may be required to perform additional monitoring, inspections or apply more stringent practices if staff determine that additional requirements are needed to assure compliance with the federal or state impaired-waters conditions. |
|---|---|
| Oil spills and ease of zardous ostances per Item o)-(c) above | Within 24 hours, an oral or electronic notification. The notification shall include information about the date, time, nature, volume and location of the spill or release. |
| Anticipated passes [40 CFR 2.41(m)(3)] | A report at least ten days before the date of the bypass, if possible. The report shall include an evaluation of the anticipated quality and effect of the bypass. |
| Unanticipated passes [40 CFR 2.41(m)(3)] | Within 24 hours, an oral or electronic notification. Within 7 calendar days, a report that includes an evaluation of the quality and effect of the bypass. |
| Noncompliance the conditions this permit that by endanger alth or the vironment [40 R 122.41(I)(7)] | Within 24 hours, an oral or electronic notification. Within 7 calendar days, a report that contains a description of the noncompliance, and its causes; the period of noncompliance, including exact dates and times, and if the noncompliance has not been corrected, the anticipated time noncompliance is expected to continue; and steps taken or planned to reduce, eliminate, and prevent reoccurrence of the noncompliance. [40 CFR 122.41(I)(6). Division staff may waive the requirement for a written report on a case-by-case basis. |

Approved Construction Plan

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance

Public Services • Engineering Division APPROVED STORMWATER MANAGEMENT PLAN Know what's **below**.

Call before you dig.

PEI JOB#: 18485.PE

Professional Seal

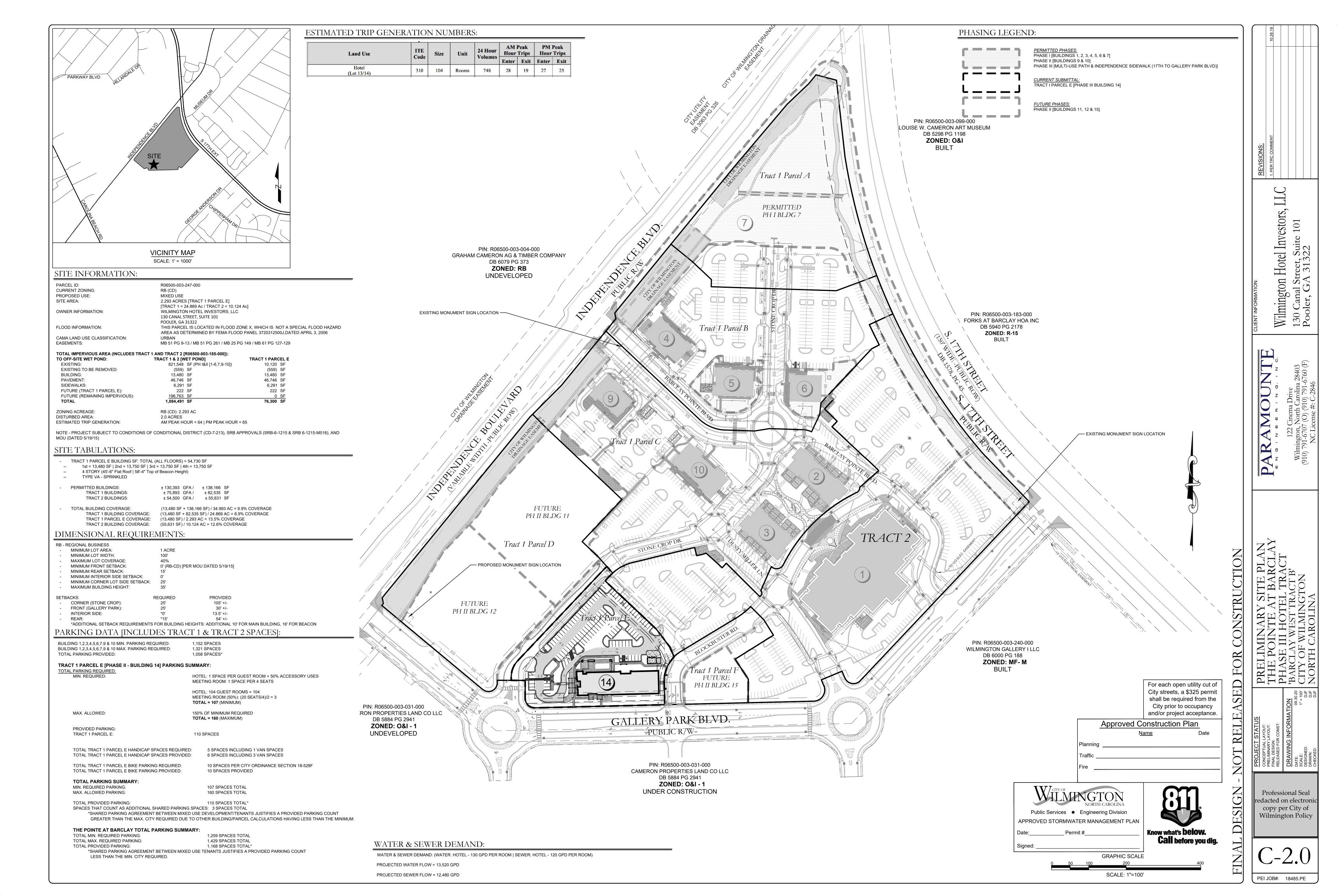
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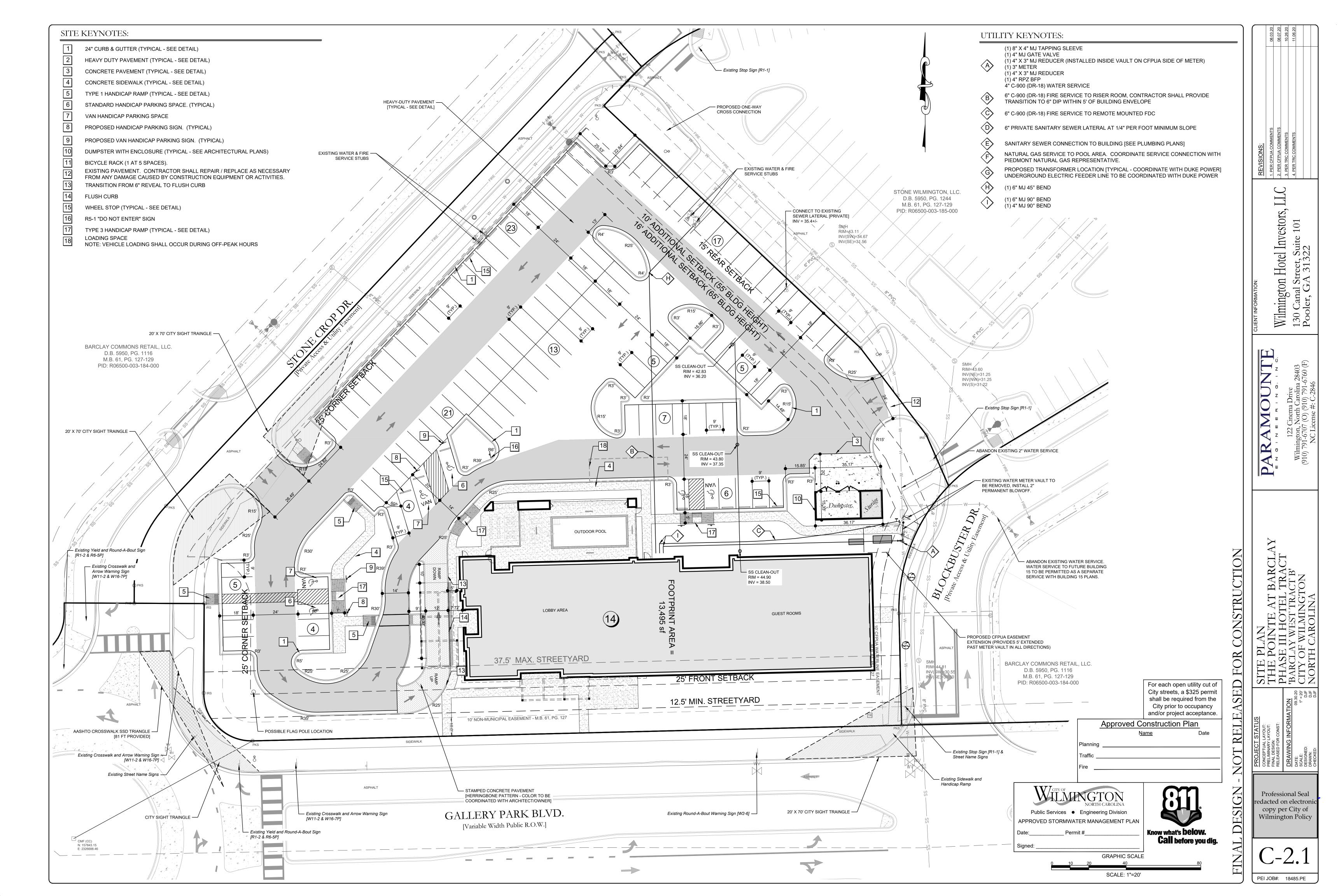
copy per City of Wilmington Policy

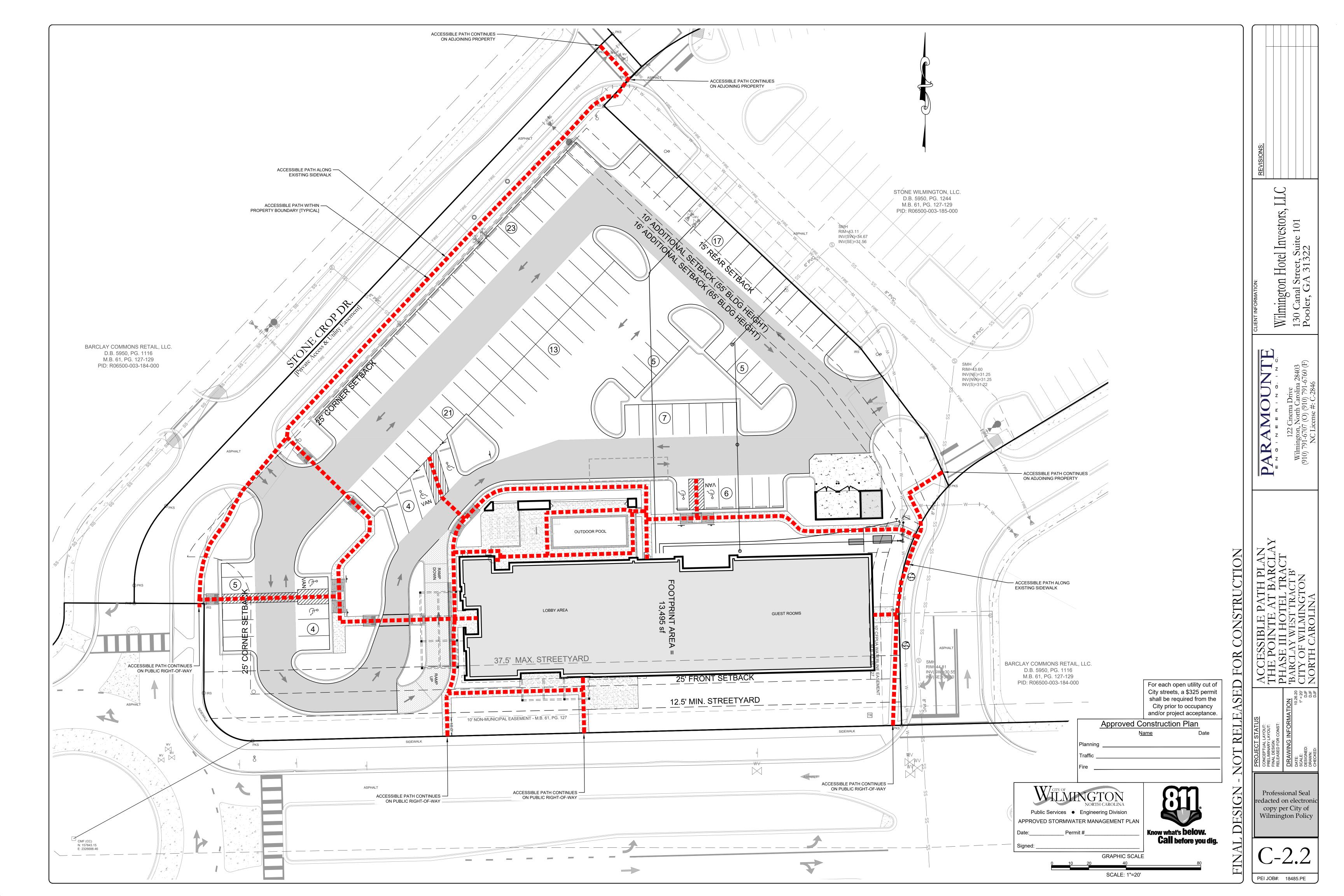
Hotel

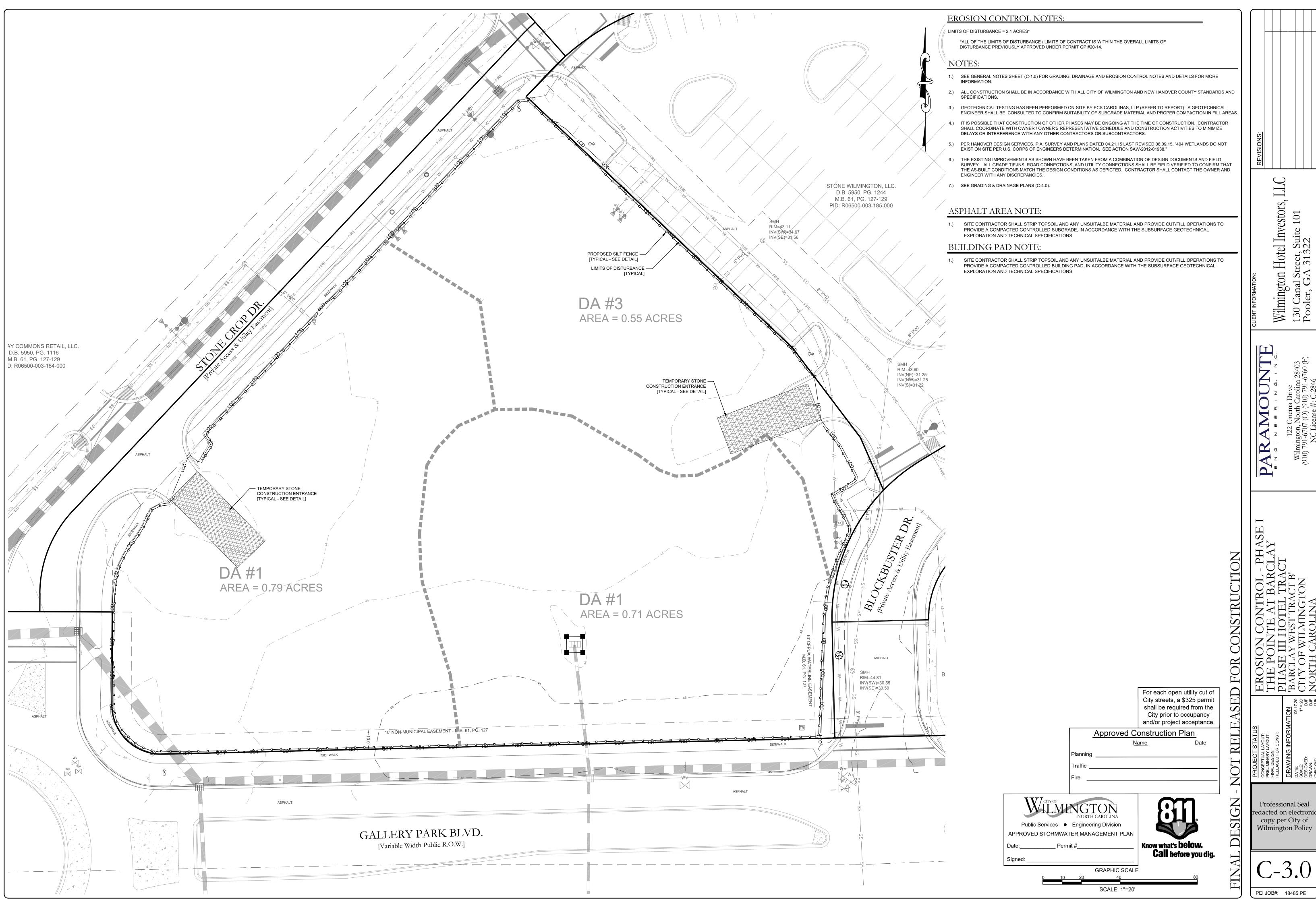
NCG01 SELF-INSPECTION, RECORDKEEPING AND REPORTING

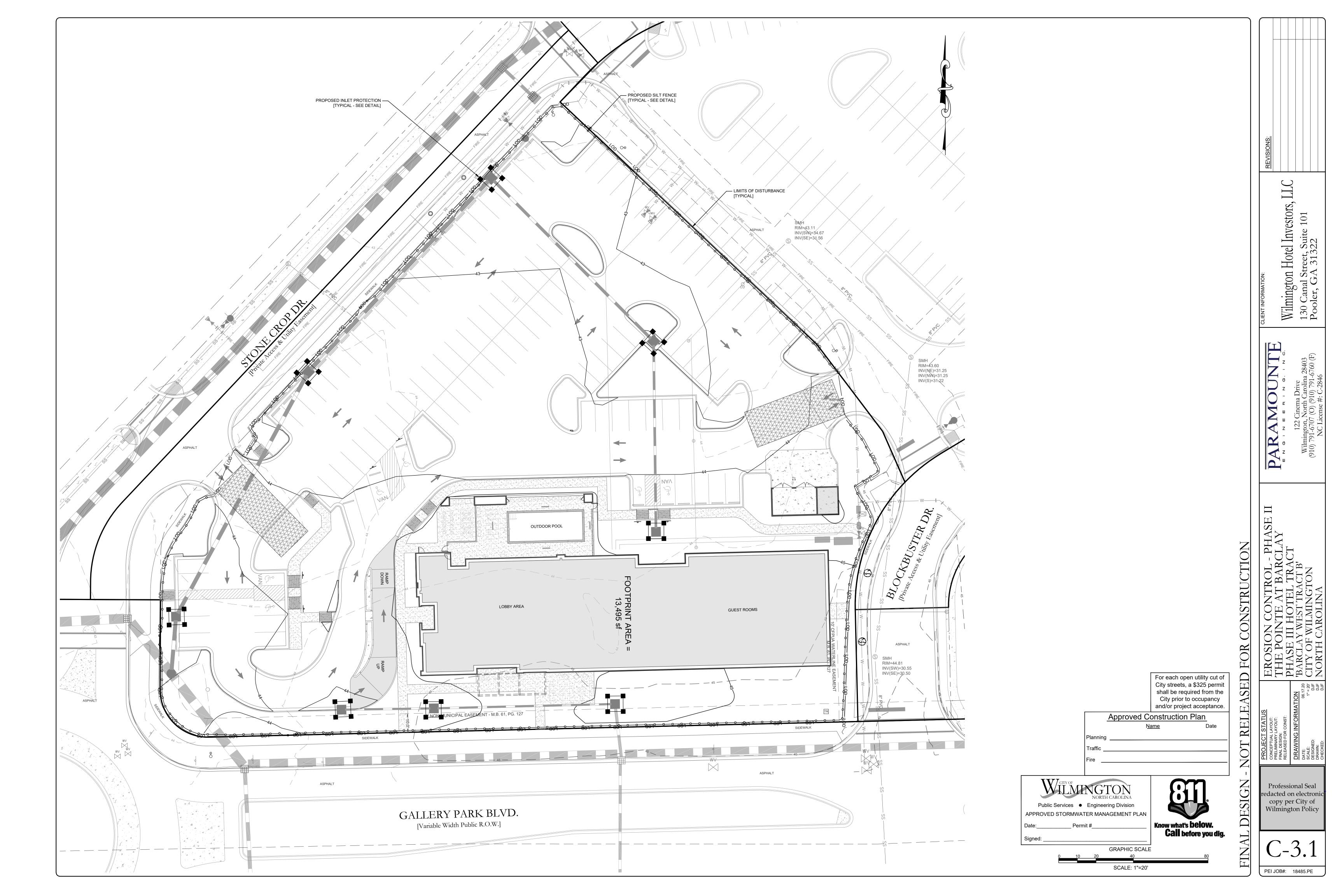
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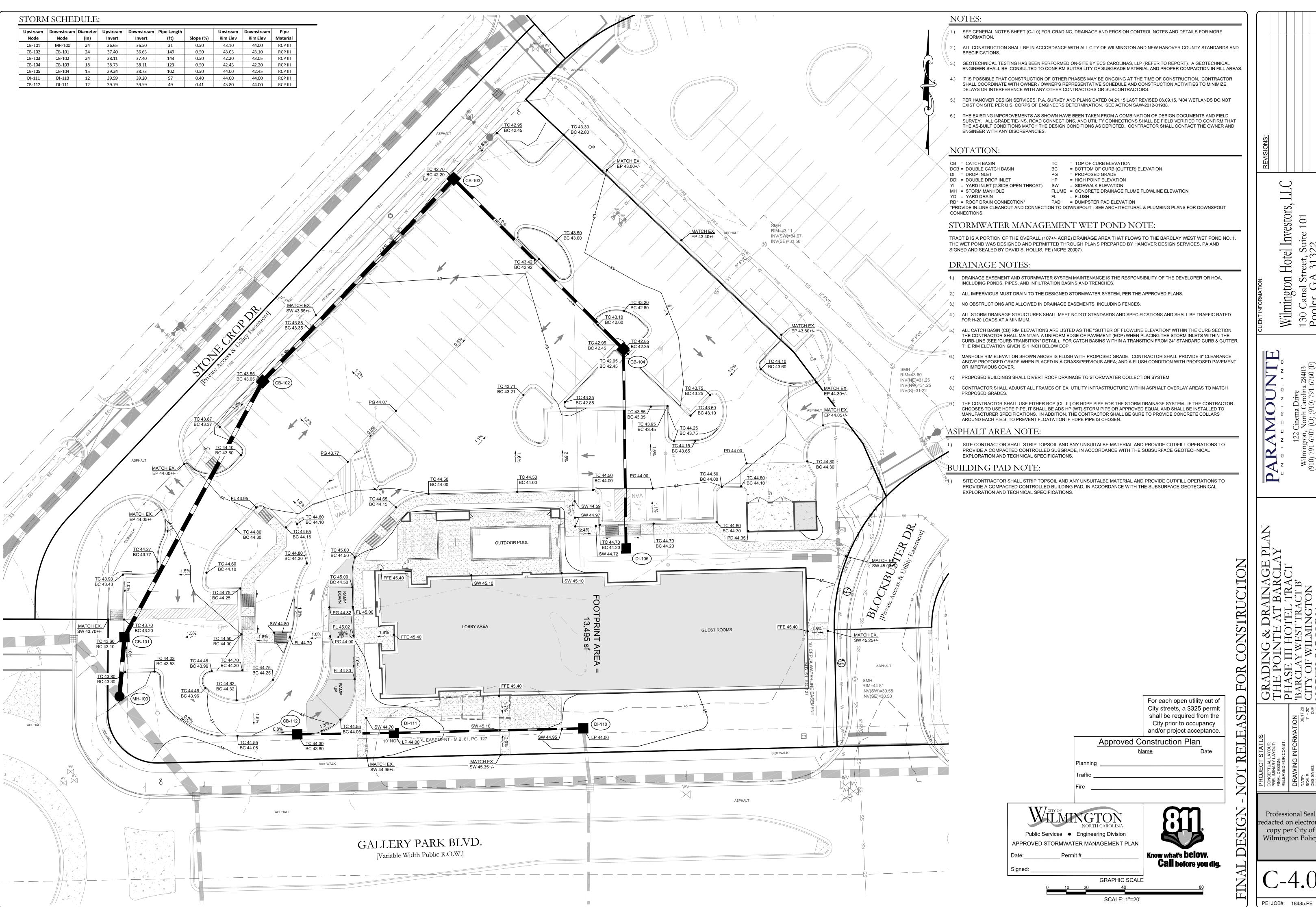




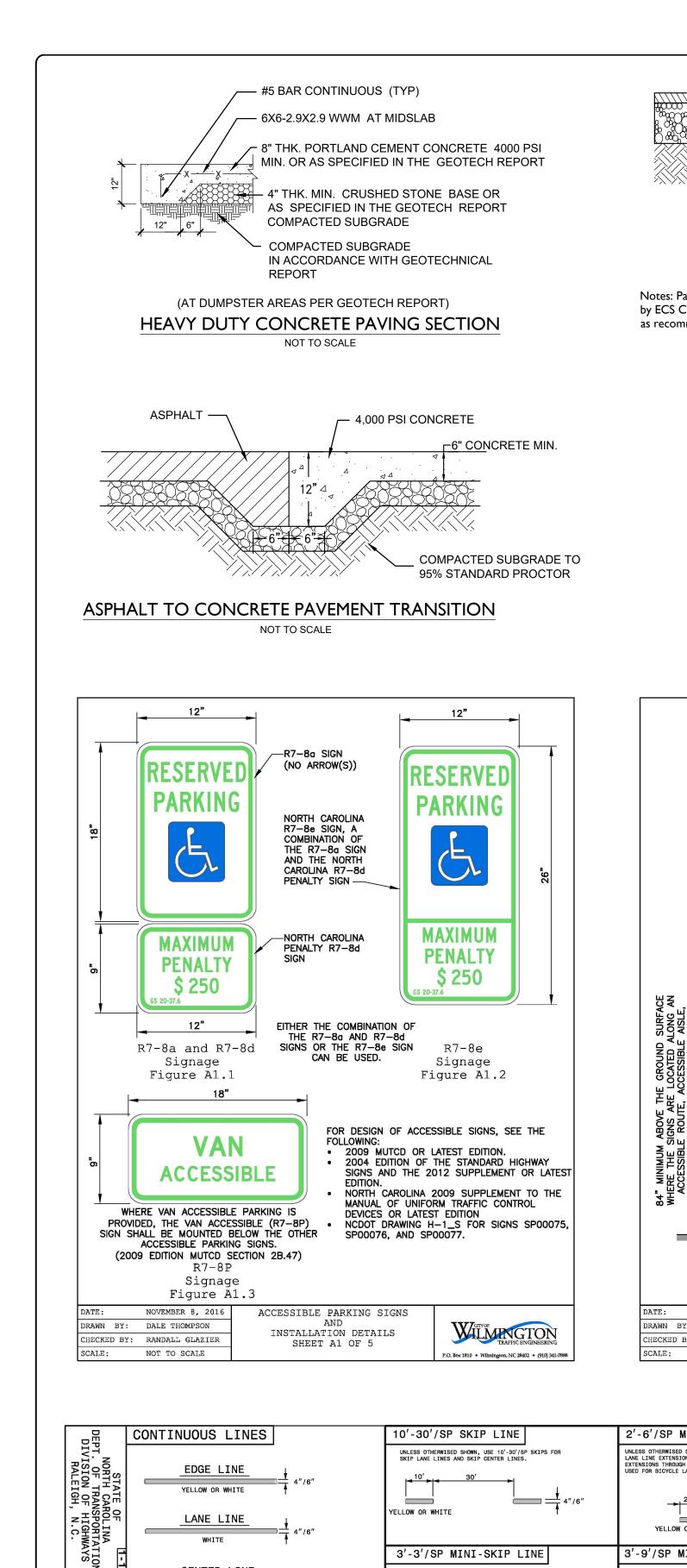


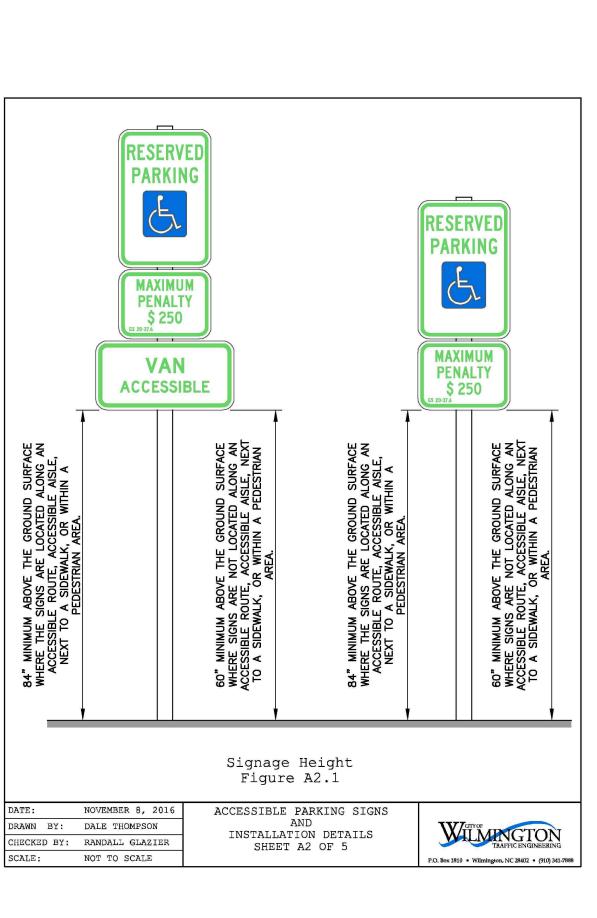


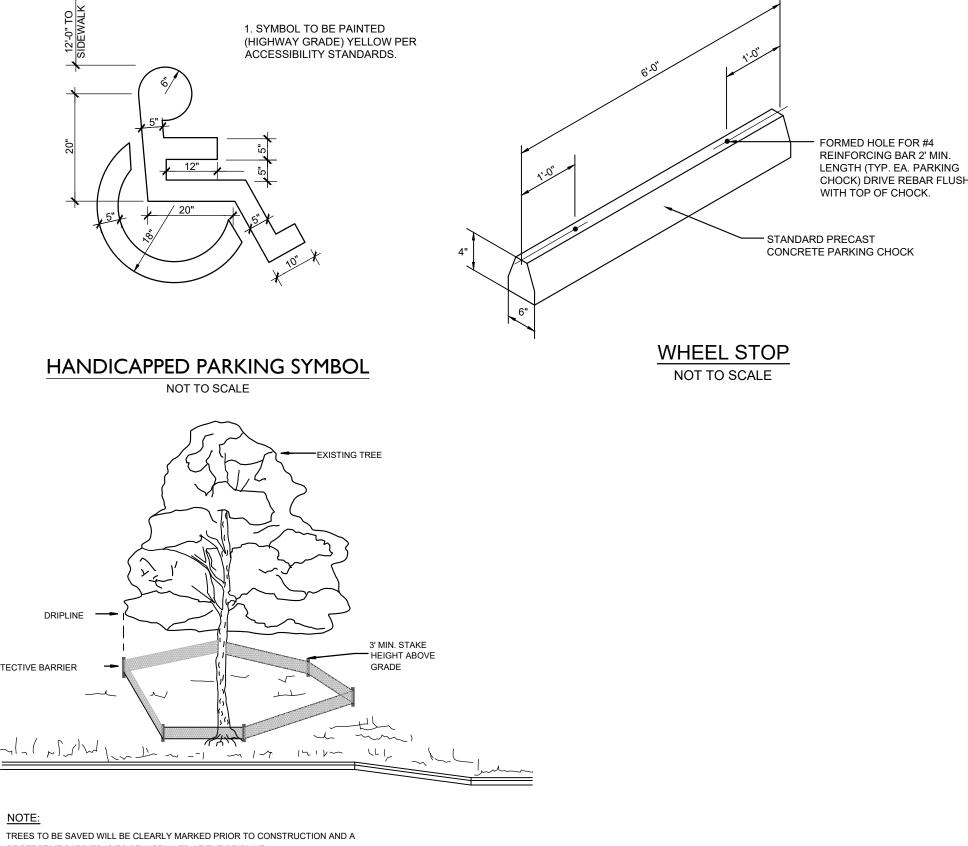


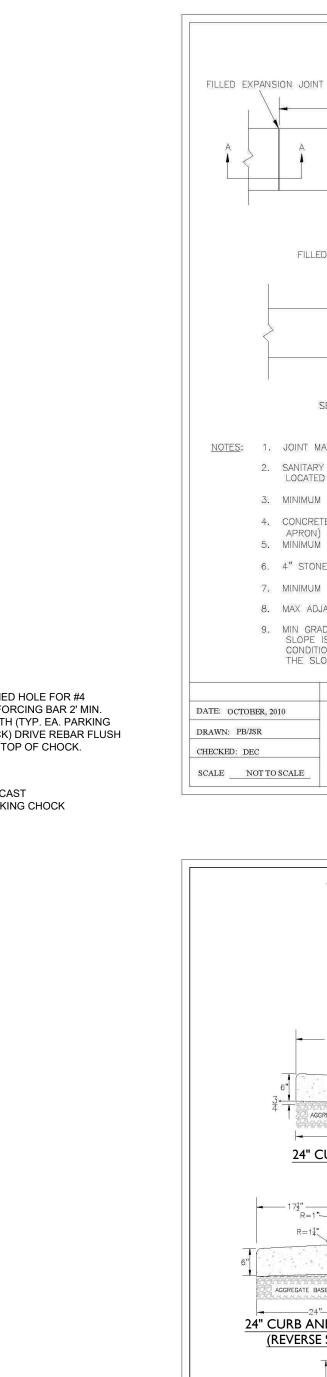


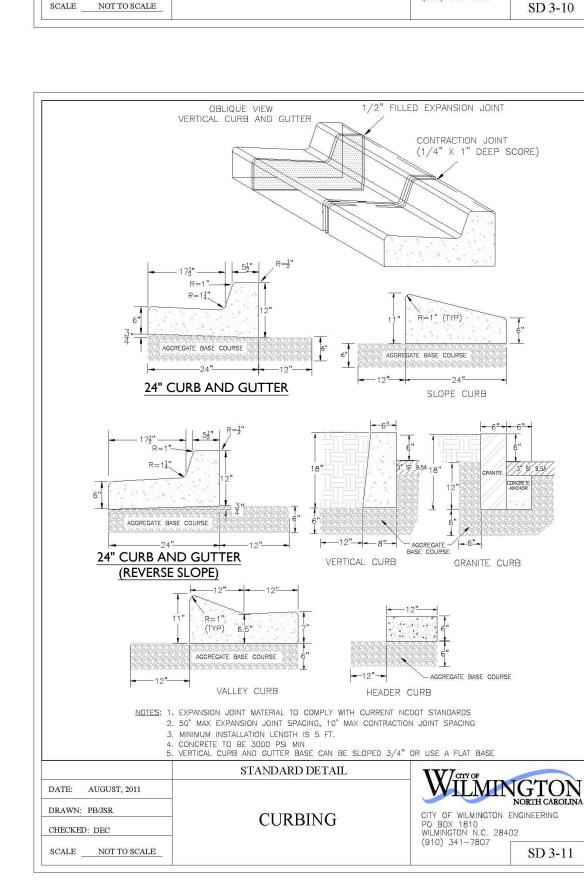
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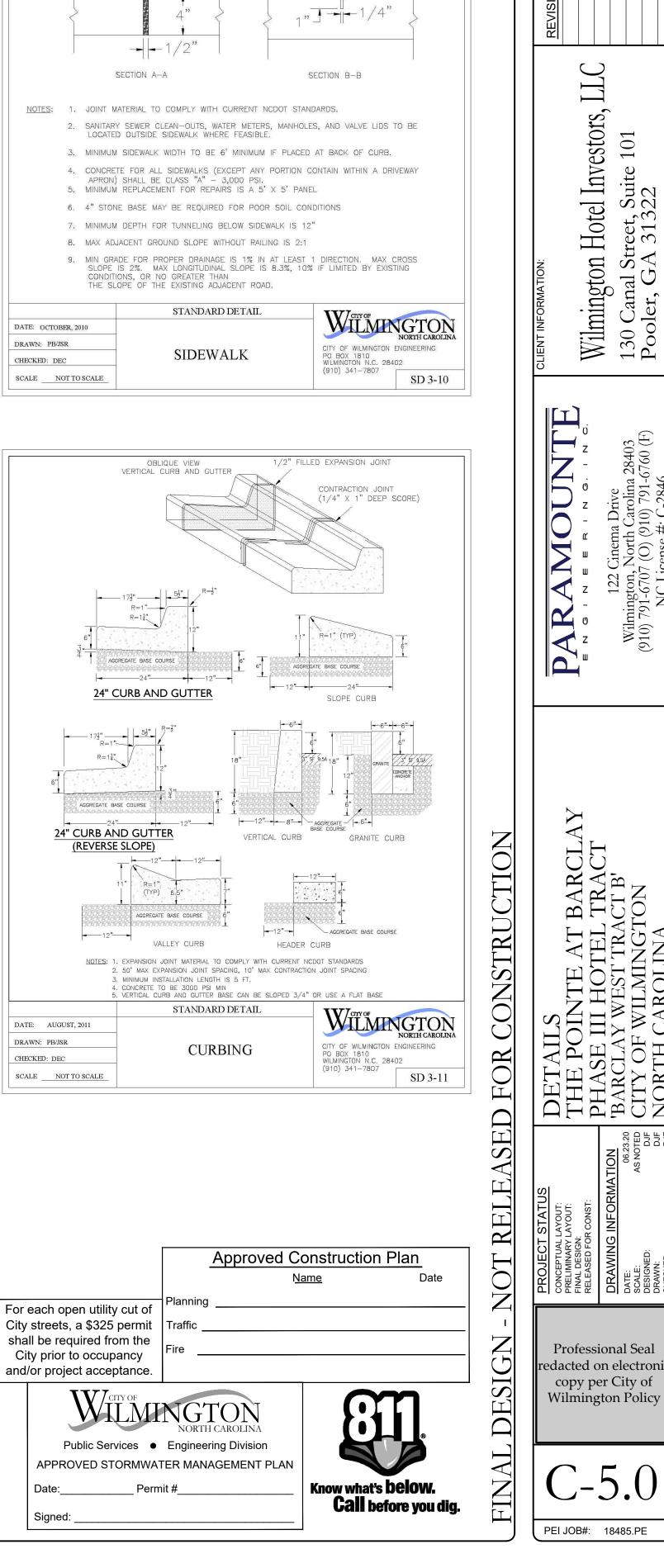








FILLED EXPANSION JOINT



FILLED EXPANSION JOINT

PEI JOB#: 18485.PE

2" S9.5A SURFACE COURSE BASE COURSE 100% ASTM D1557 COMPACTED SUBGRADE

GEOTECHNICAL REPORT

6" NCDOT CRUSHED STONE AGGREGATE IN ACCORDANCE WITH

LIGHT DUTY ASPHALT PAVING SECTION

NOT TO SCALE Notes: Pavement sections are the recommended sections per the Geotech Report prepared by ECS Carloinas, LLP and shall be augmented relative to field conditions during construction as recommended by Geotech.

HEAVY DUTY ASPHALT PAVING SECTION

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— 2" S 9.5A SURFACE COURSE

8" NCDOT CRUSHED STONE AGGREGATE

BASE COURSE 100% ASTM D1557

- COMPACTED SUBGRADE

GEOTECHNICAL REPORT

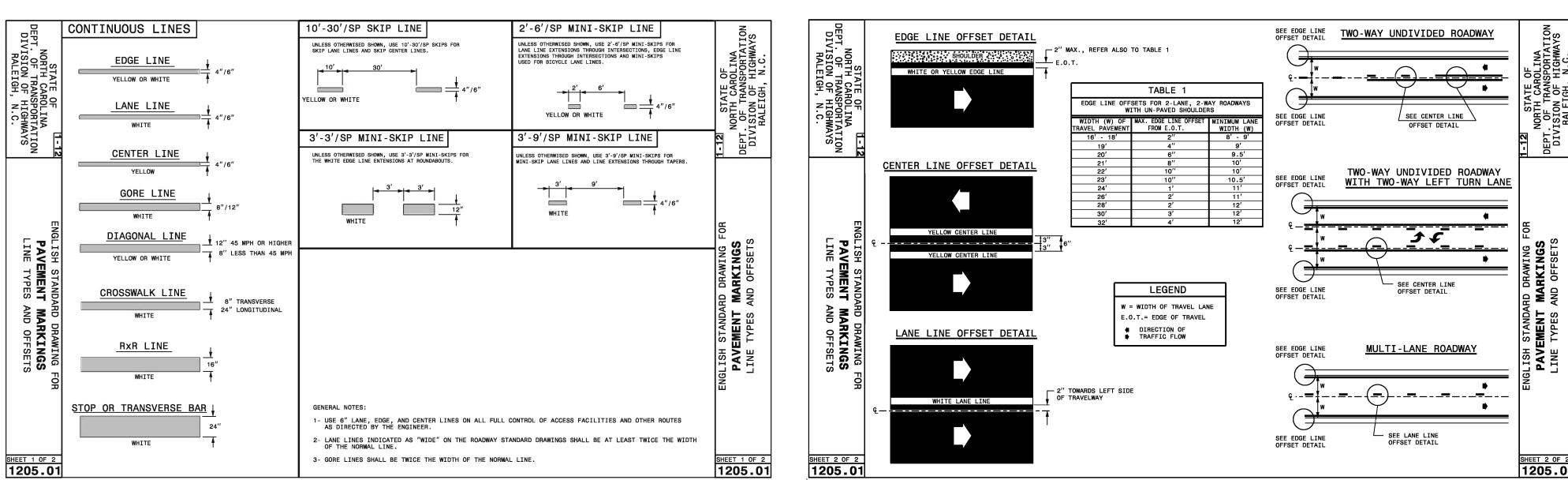
IN ACCORDANCE WITH

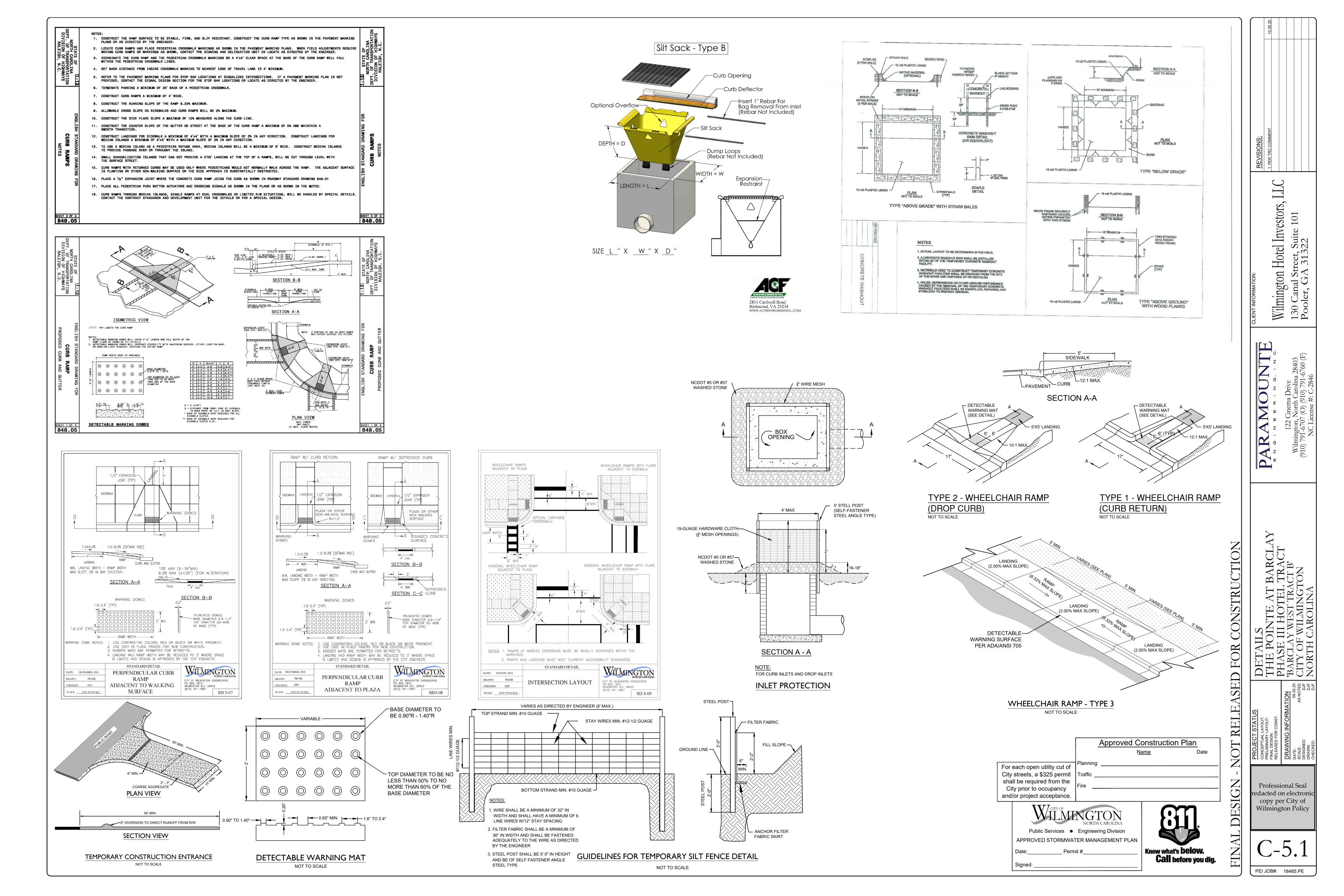
PROTECTIVE BARRIER

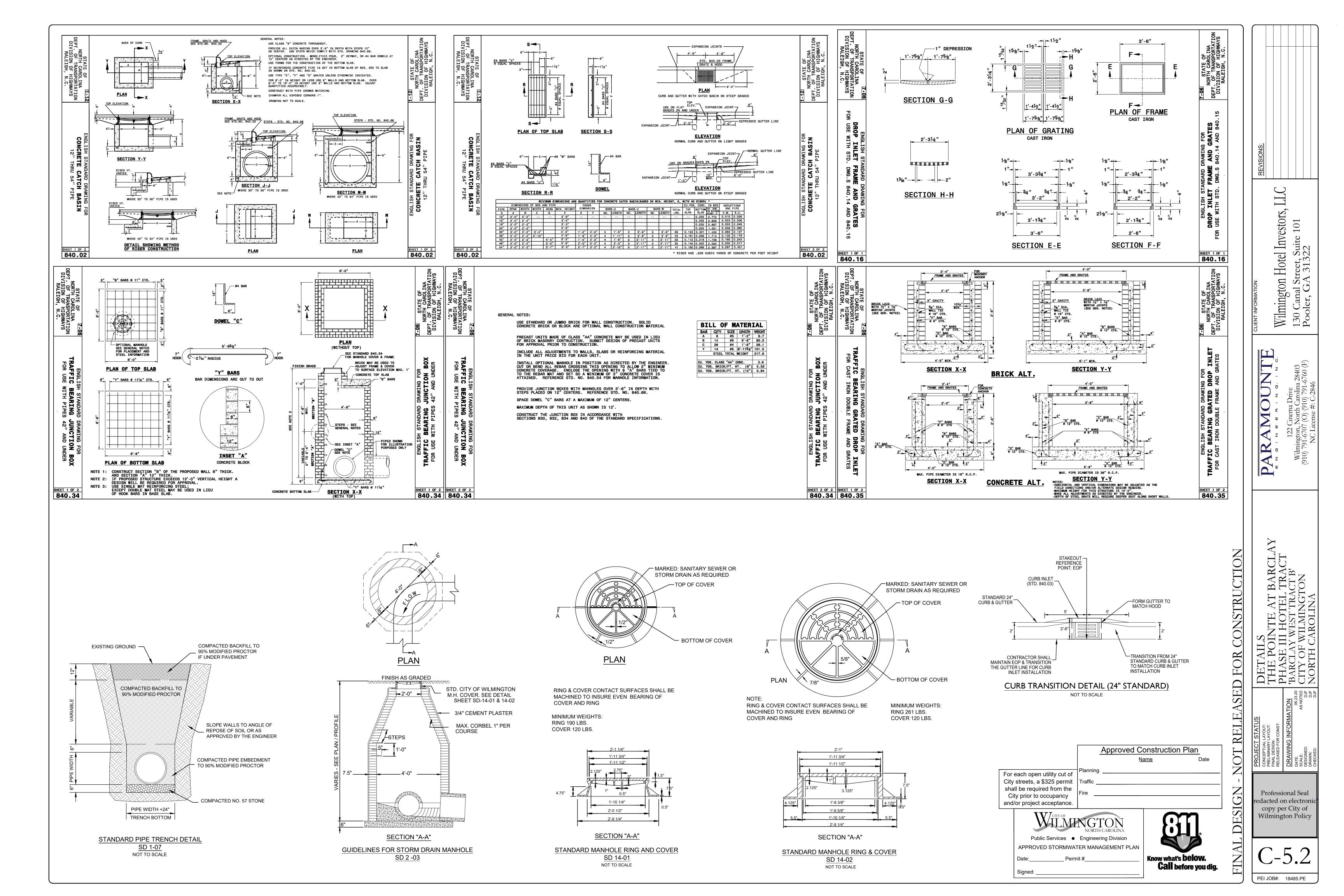
PROTECTIVE BARRIER IS TO BE INSTALLED AT THE DRIPLINE. DRIP LINE - THE AREA OF SOIL DIRECTLY BENEATH THE TREE EXTENDING OUT TO THE TIPS OF THE OUTERMOST BRANCHES.

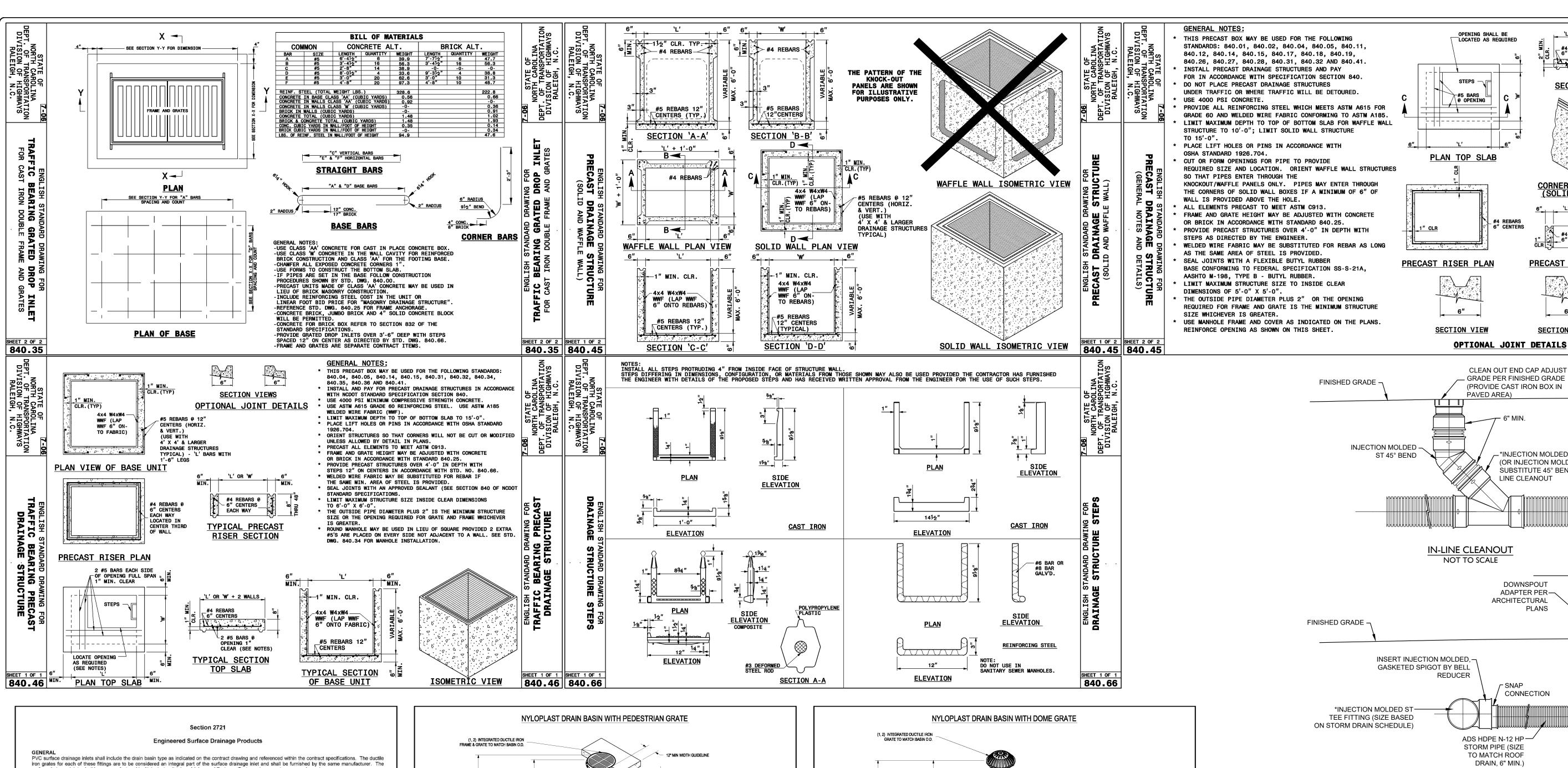
> METHOD OF TREE PROTECTION **DURING CONSTRUCTION**

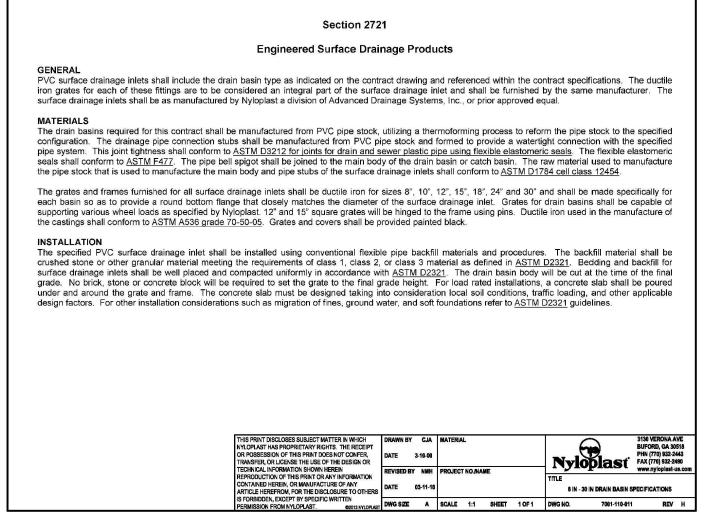
> > NOT TO SCALE

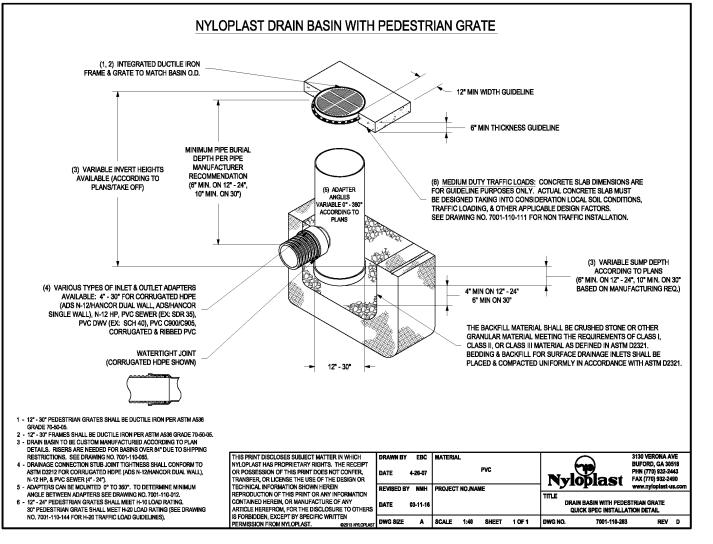


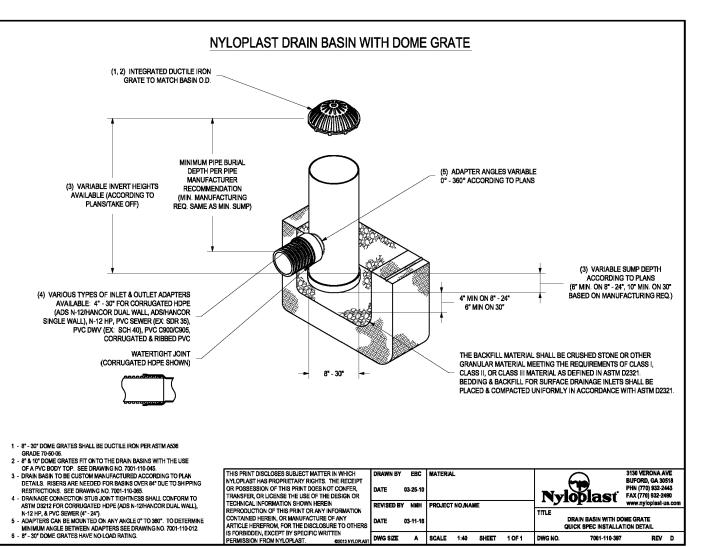




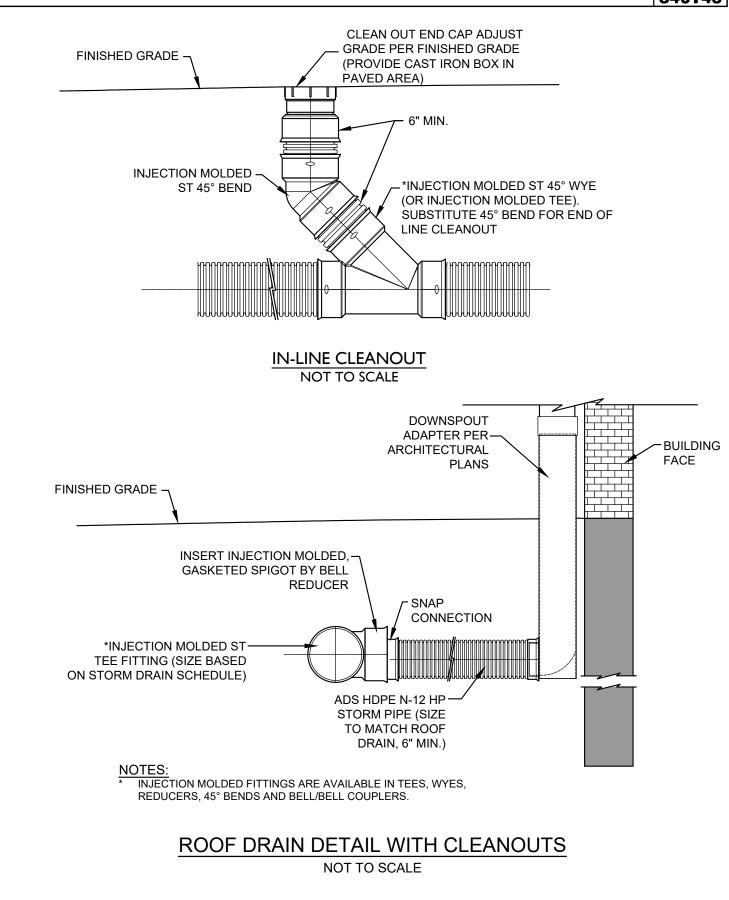








YARD INLET DETAIL (OR APPROVED EQUAL



'L' OR 'W' + 1'-0"

\\\\#5 BARS @ OPENING

SECTION 'C-C'

#4 REBARS 6" CENTERS

#4 REBARS
1" CENTERS—

SECTION VIEW

PRECAST RISER DETAI

Approved Construction Plan For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance **DESI** Public Services • Engineering Division APPROVED STORMWATER MANAGEMENT PLAN Know what's **below. Call** before you dig.

ONSTRUCTION ASEL RELE. **Professional Seal** edacted on electroni copy per City of Wilmington Policy

101

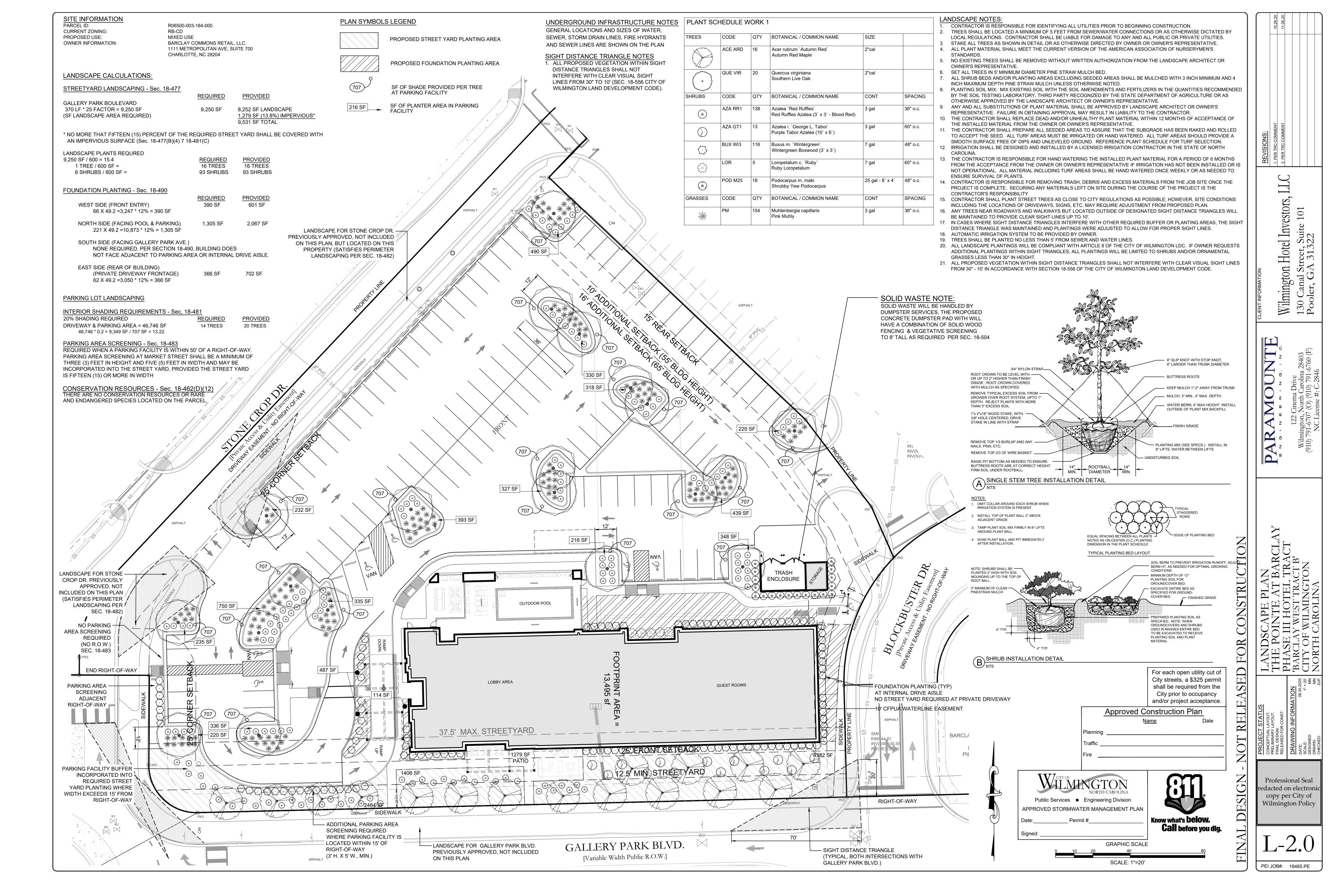
reet, Suite 31322

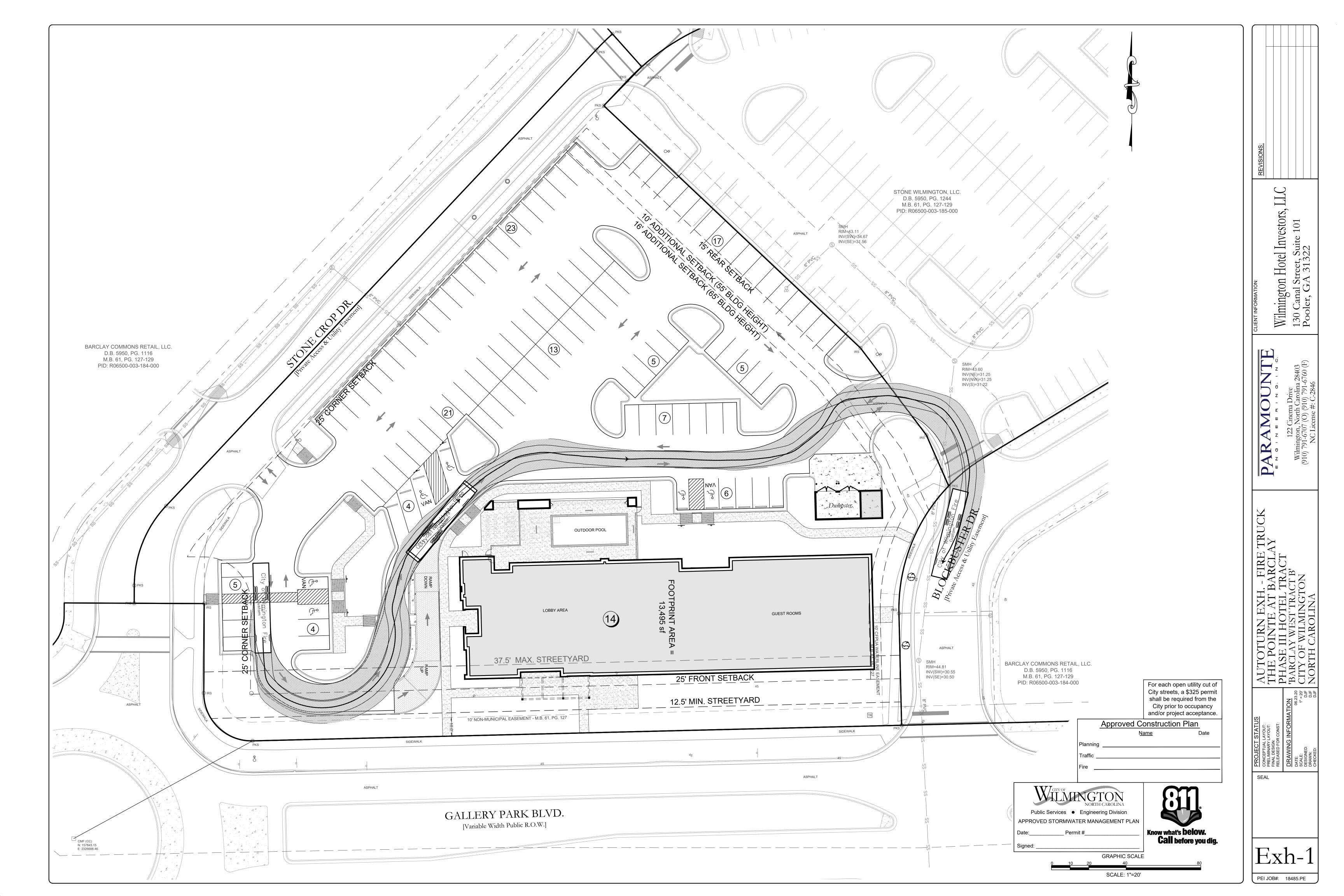
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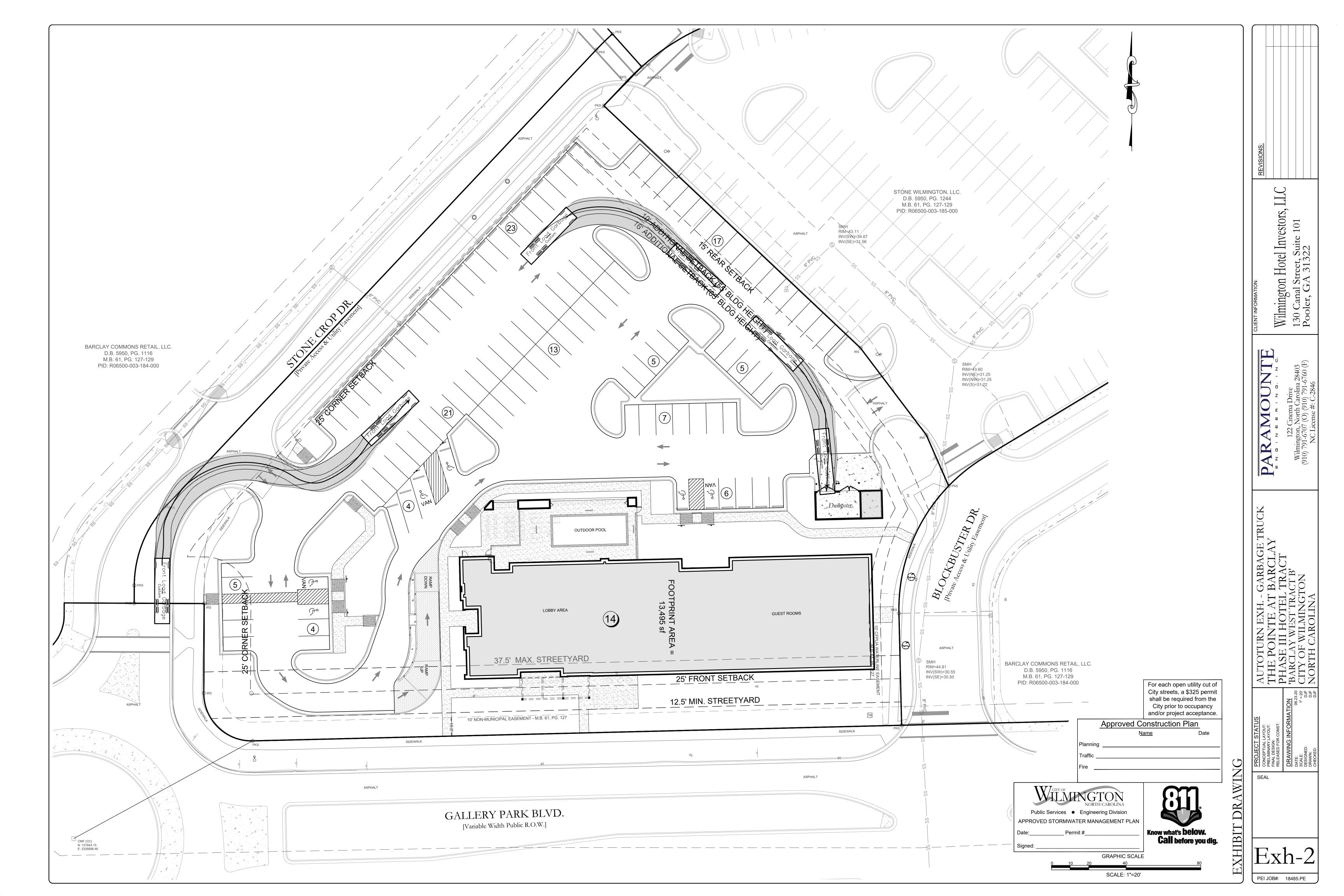
Hotel

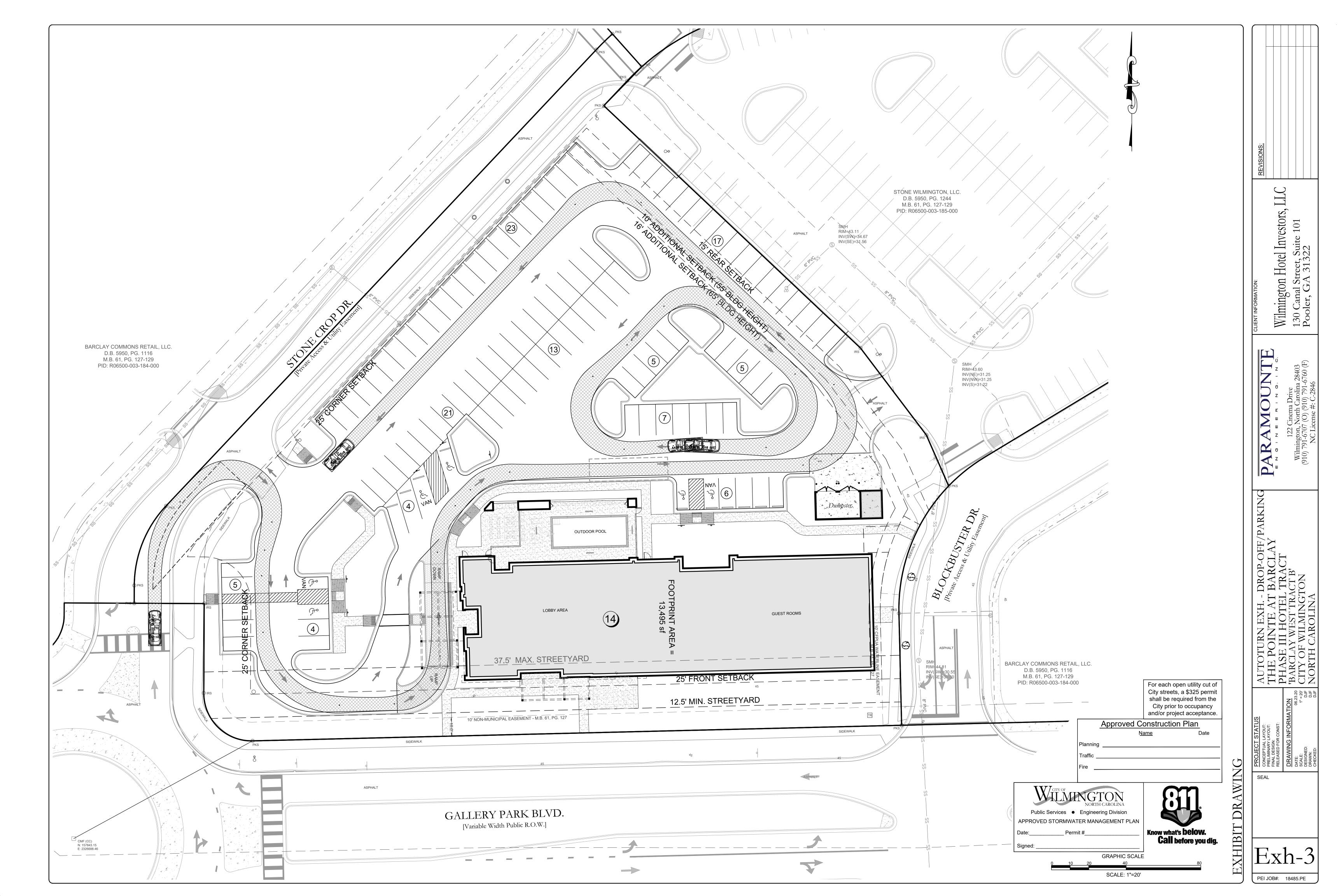
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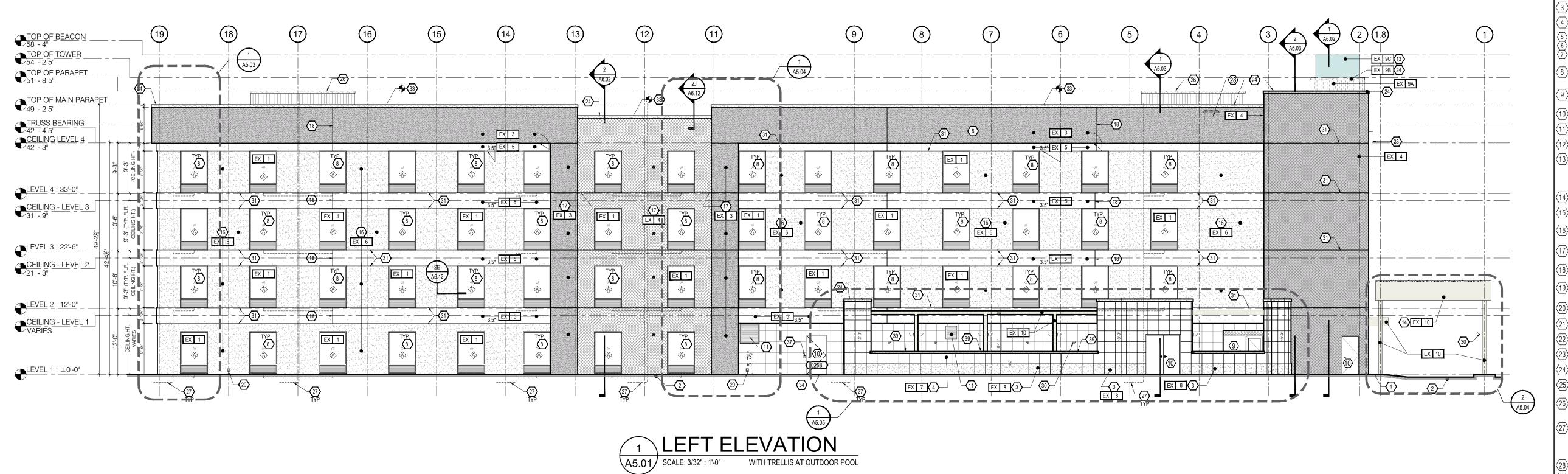
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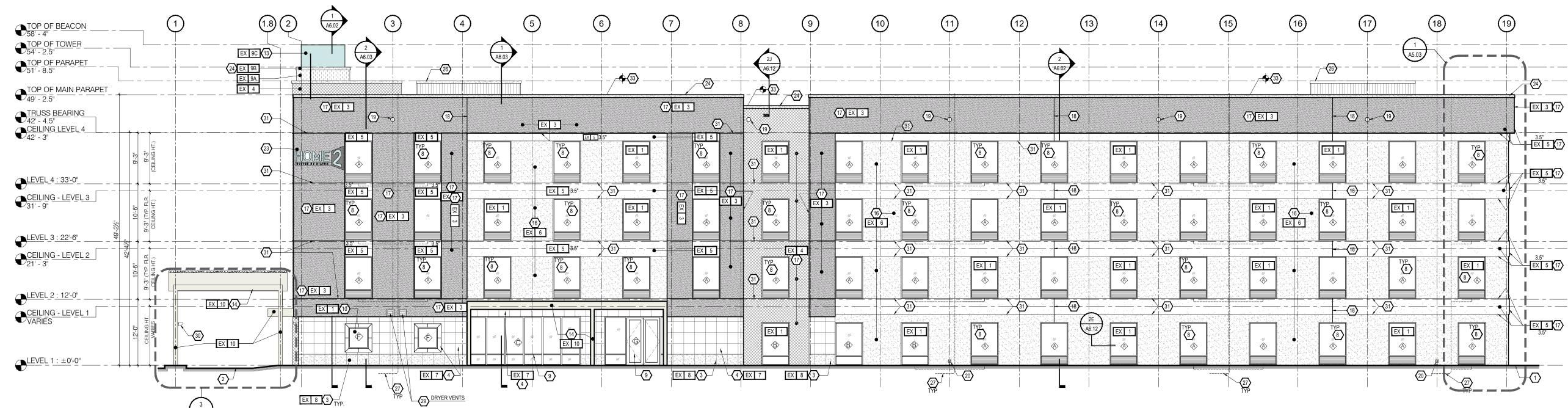












RIGHT ELEVATION A5.01 SCALE: 3/32": 1'-0" WITH ENTRANCE CANOPY & TRELLIS

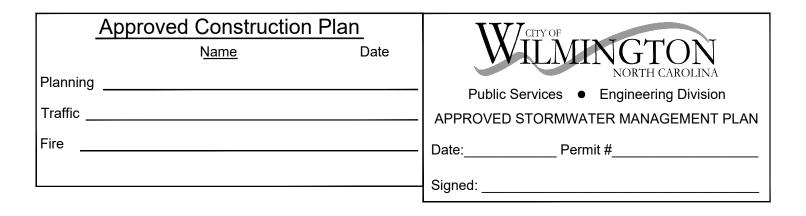
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- DESIGN CONSULTANTS AND ARCHITECTS ARE TO BE NOTIFIED IN WRITING OF ANY DISCREPANCIES. ALL WORK SHALL COMPLY TO THE INTERNATIONAL BUILDING CODE AND ALL ADOPTED
- SUB-CODES. DESIGN CONSULTANT AND ARCHITECTS ARE NOT RETAINED FOR ANY ON SITE SUPERVISION OR CONTRACT ADMINISTRATION. ALL WORK IS TO BE PERFORMED TO THE HIGHEST STANDARDS OF THE TRADES. CONTRACTOR IS TO COMPLY TO ALL STATE AND LOCAL ORDINANCES
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- SUPPORT ALL WORK AS NEW WORK PROGRESSES. THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH ALL EXISTING CONDITIONS AT THE JOB SITE PRIOR TO THE START OF ANY WORK.
- CONTRACTOR IS TO PROVIDE ACCESS PANELS AS REQUIRED NEEDED OR DIRECTED BY DESIGN CONSULTANTS, ARCHITECT, ENGINEER, CONSTRUCTION OFFICIAL OR OWNER. CONTRACTOR AND/ OR OWNER SHALL PROVIDE FOR THE SAFETY OF ALL JOB RELATED PERSONNEL AS WELL AS THE GENERAL PUBLIC.
- CONTRACTOR SHALL PROVIDE A FULL SCALE MOCKUP WALL WITH MATERIAL AND COLORS AS SPECIFIED IN THE DRAWINGS FOR THE APPROVAL OF OWNER BEFORE FINALIZING THE INSTALLATION OF SAID MATERIAL TO BUILDING.

AIR & MOISTURE BARRIER NOTES

- OVERLAP ALL HORIZONTAL AND VERTICAL SEAMS BY 6" MINIMUM. TAPE AND SEAL ALL SEAMS, BOTH VERTICAL AND HORIZONTAL WITH TAPE APPROVED BY AIR AND MOISTURE BARRIER MANUFACTURER.
- PROVIDE SPRAY APPLIED AIR TIGHT FOAM INSULATING AIR SEAL ALL AROUND DOOR/WINDOW FRAMES. SEAL ALL GAPS AND PENETRATIONS FROM INSIDE AND OUTSIDE THE BUILDING . PROVIDE CONT. FLASHING WITH BACKER ROD AND SEALANT AT WINDOW SILL AND LINTEL . FILL ALL AIR GAPS AT WINDOW JAMB WITH SPRAY APPLIED FOAM INSULATION

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- ANY LUMBER IN CONTACT WITH CONCRETE WOULD BE PRESSURE TREATED (P.T.). PROVIDE FLASHING AND BACKER ROD WITH SEALANTS AS SHOWN AND AS REQUIRED. IF ANY PLACE DRAWING NOT SHOWN AND IS REQUIRED PER SITE CONDITIONS, CONTRACTOR WILL PROVIDE THEM WITH "NO" ADDITIONAL COST TO THE OWNER.
- GROUND MOUNTED CONDENSING UNITS SHOULD BE SCREENED WITH MATERIAL AS REQUIRED AND APPROVED BY HILTON. . ALL UTILITY METERS (GAS, WATER AND ELECTRICAL METERS) SHOULD BE SCREENED IF REQUIRED BY CITY. USE MATERIAL AND FINISH AS APPROVED BY HILTON. . ALL LOUVERS AND WALL CAP SHOULD BE PAINTED TO MATCH ADJACENT WALL COLOR
- 10. ANY ITEMS/SYSTEM NOT SHOWN ON THE DRAWINGS THAT MAY BE REQUIRED DURING CONSTRUCTION WOULD BE PROVIDED BY THE CONTRACTOR WITHOUT ANY ADDITIONAL COST TO

KEY NOTES:

- APPROXIMATE LINE OF GRADE
- DRIVEWAY AND DROP-OFF BELOW CANOPY
- MASONRY OR TILE BASE
- MASONRY OR TILE FIELD ASHLAR PATTERN
- NOT USED NOT USED
- ALUMINUM FIXED WINDOW W/ THERMAL BROKEN FRAME. INSULATED GLAZING WITH INTEGRAL ALUMINUM LOUVER AT PTAC UNITS. SEE SCHEDULE FOR GLASS TYPE.
- ALUMINUM STOREFRONT SYSTEM W/ THERMAL BROKEN FRAME AND INSULATED GLAZING. SEE SCHEDULE FOR GLASS TYPE.
- INSULATED HOLLOW CORE METAL DOOR. SEE DOOR WINDOW SCHEDULE
- MECHANICAL LOUVER, REFER TO MECHANICAL DRAWINGS
- ALUMINUM SLIDING ENTRY DOOR W/ INSULATED GLAZING
- BEACON: GLASS CROWN: MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR

POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE. - REFER TO

- PAINTED TUBE STEEL CANOPY W/ STAINED WOOD TRELLIS
- DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOF
- DRAINABLE FIES FINISH ON FPS FOAM OVER WEATHER BARRIER AND EXTERIOR GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOR
- OVERFLOW SCUPPER -BASIS OF DESIGN NESCO MFG INC
- MODEL # SCT914R -- REFER ALSO TO DETAIL 6J/A6.12 AMPER RESISTANT, RECESSED HOSE BIB - MOUNT TOP AT +12" A.F.F.
- ENVELOPE ACCENT FINISH -REFER TO STYLE GUIDE
- ENVELOPE ACCENT FINISH -REFER TO STYLE GUIDE

- KYNAR FINISH ALUMINUM COPING SYSTEM --COLOR TO MATCH ADJACENT MATERIAL
- REVIEW ALL VIEWS AROUND PROPERTY BEFORE LOCATING ALL ROOF TOP MECHANICAL UNITS. VERIFY ALL EQUIPMENT IS ADEQUATELY SCREENED
- PTAC CONDENSATE DRAIN RISER IN EXTERIOR WALL. ALL STACKED PTAC UNITS TO BE CONNECTED TO A VERTICAL STACK PIPE WHICH SHOULD DRAIN TO NEAREST STORM WATER PIPE OR INLET. GC TO PROVIDE VENTS AND TRAPS AS REQUIRED FOR THE RISER (RISER SHOWN ON ELEVATION ARE TYPICAL AND SHOULD BE PROVIDED AT ALL PTAC UNITS)
- AINTED LINEN CHUTE, COLOR TO MATCH ADJACENT ENVELOPE
- DRIER EXHAUST VENT -- COLOR TO MATCH ADJACENT MATERIAL
- LIGHT FIXTURE --REFER TO CEILING PLAN
- EXPANSION JOINT @ FLOOR LINE AND WINDOW HEAD W/ BACKER ROD AND SEALAN' ALUMINUM LOUVER -- COLOR TO MATCH ADJACENT MATERIAL MATCHING ADJACEN⁻
- THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION. NTERIOR CEILING HEIGHT DIMENSIONS ARE CRITICAL TO MAINTAIN AS MINIMUMS.
- CONCRETE PAD REFER TO AREA DEVELOPMENT PLAN
- ALUMINUM GUTTER AND DS. COLOR TO MATCH ADJ. MATERIAL
- ACCESSIBLE KEY CARD READER WIRED TO ELECTRIC STRIKE IN DOOR TOP OF READER AT 48" MAX. HEIGHT
- POOL PERIMETER FENCE AS REQUIRED BY CODE W/ 3'-0" GATE WITH PANIC/EXIT HARDWARE
- PRECAST COPING
- HI-LO DRINKING FOUNTAIN
- EMERGENCY PHONE
- ADA COMPLIANT STANDING SHOWER. PROVIDE TILE WALL AT SHOWER

FINISH KEY: ENVELOPE - ACCENT METAL - ALUMINUM COLOR WHITE

- X 2 NOT USED EX 3 ENVELOPE - WRAP - EIFS, COLOR: BENJ. MOORE AC-22, NANTUCKET FOG ENVELOPE - LINK & KEEP - EIFS, COLOR: BENJ. MOORE HC-70 VAN, BUREN
- BROWN EX | 5 | ENVELOPE - ACCENT - EIFS, COLOR: BENJ. MOORE 2111-50, STONE HARBOR
- EX 6 ENVELOPE CORE EIFS, COLOR: BENJ. MOORE 2109-40, SMOKED OYSTER ENVELOPE - SUPPORT - CAST STONE. ARRISCRAFT RENAISSANCE -SERIES GINGER-SMOOTH FINISH
- ENVELOPE SUPPORT CAST STONE. ARRISCRAFT RENAISSANCE -SERIES GINGER-ROCKED FINISH BEAM: VERTICAL ELEMENT
- SET BACK IN PLANE FROM THE KEEP, EXTENDS TO THE GROUND. COMPOSED OF THREE ELEMENTS EX 9A REVEAL: VERTICAL ELEMENT EXTENDING THROUGH THE KEEP, EIFS, COLOR: BENJ. MOORE 398, FLOWER POWER
- EX 9B BAND: HORIZONTAL ELEMENT CAPPING THE REVEAL EIFS, COLOR: BENJ. MOORE 398, FLOWER POWER GLASS CROWN MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT
- WHITE GLASS OR POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE. EX 10 ENVELOPE - CANOPY - STEEL, COLOR: BENJ. MOORE OC-17, WHITE DOVE
- EIFS TYPE/THICKNESS NOTE:
 INDICATES EIFS NUMBER WITH

INDICATES EIFS NUMBER WITH CORRESPONDING COLORS SHOWN ABOVE INDICATES EIFS THICKNESS. IF NO NUMBER IS SHOWN, THE EIFS IS 2" NOMINAL THICK

BUILDING SIGNAGE:

• ALL SIGNAGE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. LOCATION SIZE, ETC. TO BE DETERMINED ON AN INDIVIDUAL PROJECT BASIS. • MINIMUM 3/4" PLYWOOD BACKER-BOARD REQUIRED AT SIGN LOCATIONS. AREA SHOULD COVER ENTIRE LENGTH AND HEIGHT OF FASCIA OR SPACE AVAILABLE FOR SIGN. • ELECTRICAL AND FINAL CONNECTION BY CONTRACTOR. ELECTRICAL REQUIREMENTS MAY BE OBTAINED FROM SIGN COMPANY. TYPICAL 120V REQUIREMENT.

• RACEWAYS/ WIREWAYS ARE NOT ALLOWED. PERMANENT ACCESS DOORS TO INTERIOR OF ALL PARAPETS WHERE SIGNS ARE LOCATED TO BE PROVIDED BY CONTRACTOR. CONTRACTOR TO FURNISH AND CONNECT PRIMARY ELECTRICAL. SERVICE INSIDE PARAPET WALL

GUESTROOMS OR PUBLIC SPACES WITH SIGNAGE ON EXTERIOR WALLS MUST HAVE ELECTRICAL PENETRATIONS AND/ OR INSTALLED PRIOR TO CLOSING UP THE WALL. PENETRATION LAYOUT MA BE OBTAINED FROM SIGN COMPANY.

GENERAL NOTES

- REFER TO HOME 2 SUITES BY HILTON STANDARDS MANUAL GUIDE FOR ADDITIONAL INFORMATION REGARDING EXTERIOR MATERIALS, CONSTRUCTION REQUIREMENTS AND SIGNAGE REQUIREMENTS.
- GROUT COLOR TO MATCH EXTERIOR TILE OR MASONRY.
- BUILDING HEIGHT DIMENSIONS ARE BASED ON WOOD FRAME CONSTRUCTION. THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION. INTERIOR CEILING HEIGHTS ARE CRITICAL TO MAINTAIN AS MINIMUMS.
- EIFS FINISH SYSTEM OVER MIN. 2" NOMINAL THICK EPS ON EXTERIOR SHEATHING AND APPROVED BUILDING WRAP. APPROVED MANUFACTURER PER HILTON SPECS - "WATER MASTER DRAINABLE SYSTEM" OR EQUAL TO BE USED. SEE MANUFACTURER'S SPECS. FOR NSTALLATION DETAILS.



ILMMPHT ID #55106

WILMINGTON, NC. 19-088ND



2885 LAWRENCEVILLE SUWANEE ROAD SUITE C, SUWANEE. GEORGIA TEL: (404) 740-7589 info@omdesignsolutions.com

STRUCTURAL

STABILITY ENGINEERING 1376 Church St., Ste 200, Decatur, GA 30030 Ph/Fax: 404-377-9316

4360 Chamblee Dunwoody Road, Suite 210 Atlanta, GA 30341

P (770) 451-6757, F (770) 451-6761

PARAMOUNTE ENGINEERING INC. 122 CINEMA DRIVE WILMINGTON, NC. 28403 PHONE: 910-791-6707

OWNER

MR. MEHUL PATEL SUITE 101, 130 CANAL STREET, POOLER, GA 31322

THE DOCUMENT, ARRANGEMENTS, ANNOTATIONS AND GRAPHICAL PRESENTATION: ON THIS DOCUMENTS ARE THE PROPERTY OF "OM DESIGN SOLUTIONS - DESIGN CONSULTANTS" WHO RETAINS OWNERSHIP AND AUTHORSHIP OF THIS DOCUMENT I

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REVISION # DATE

| DATE | ISSUED TO | ISSUED FOR |
|------------|-----------|-------------------|
| 10-14-2019 | OWNER | FLOOR PLAN REVIEW |
| 01-23-2020 | HILTON | 25% REVIEW |
| 10-16-2020 | HILTON | 60% SUBMITTAL |
| | | |

REMARKS

PROJECT ADDRESS: **HOME2 SUITES** BY HILTON **BARCLAY WEST TRACT B**

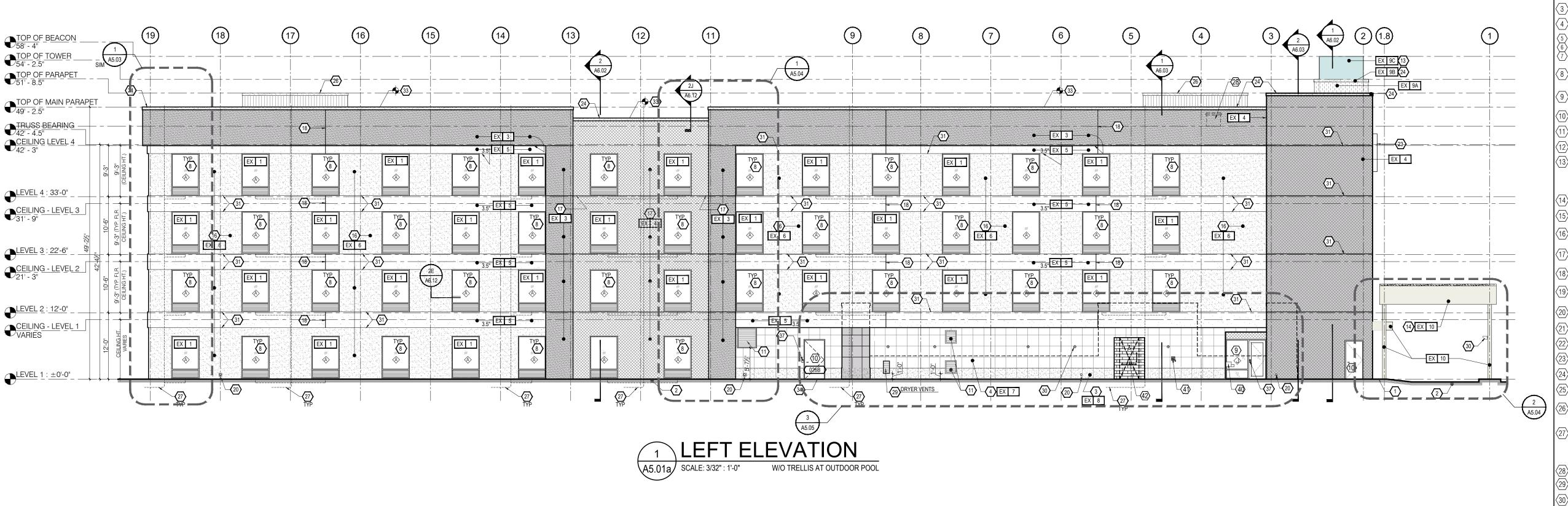
WILMINGTON, NORTH CAROLINA

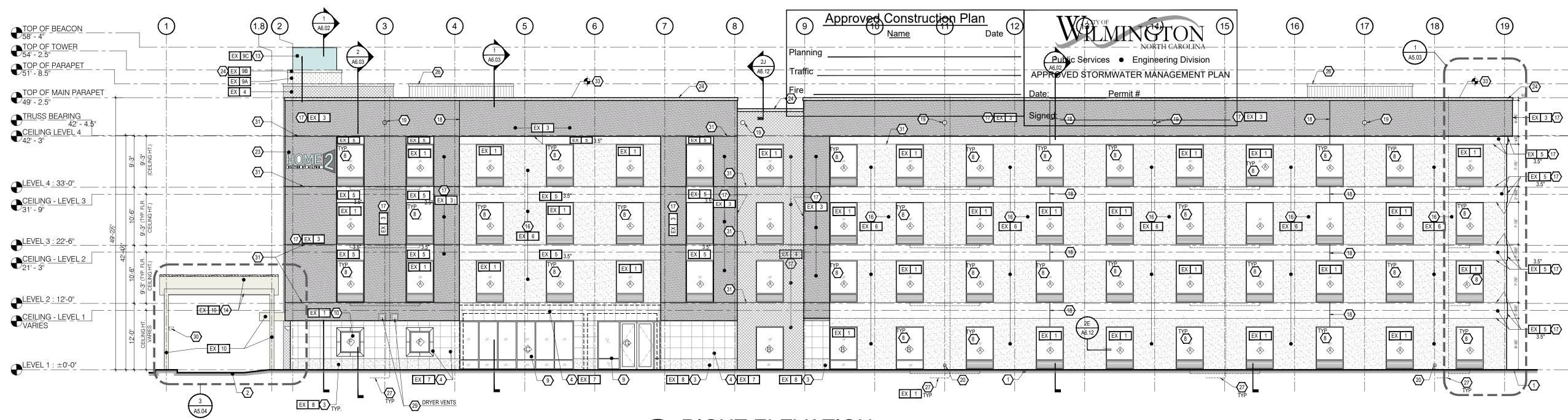
BUILDING ELEVATION

PAPER SIZE: 24X36 (ARCH D)

SCALE: AS NOTED PROJECT NUMBER:

SHEET NUMBER:





² RIGHT ELEVATION A5.01a SCALE: 3/32": 1'-0" W/O ENTRANCE CANOPY & TRELLIS

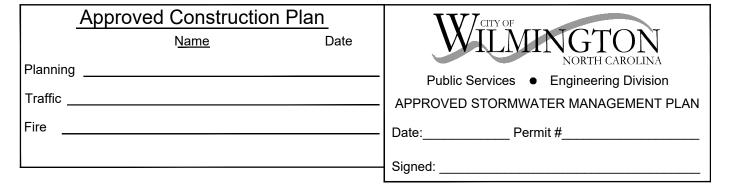
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MASONRY OR TILE - FIELD - ASHLAR PATTERN

- NOT USED NOT USED
- ALUMINUM FIXED WINDOW W/ THERMAL BROKEN FRAME. INSULATED GLAZING WITH INTEGRAL ALUMINUM LOUVER AT PTAC UNITS. SEE SCHEDULE FOR GLASS TYPE.
- ALUMINUM STOREFRONT SYSTEM W/ THERMAL BROKEN FRAME AND INSULATED GLAZING. SEE SCHEDULE FOR GLASS TYPE.
- INSULATED HOLLOW CORE METAL DOOR. SEE DOOR WINDOW SCHEDULE
- MECHANICAL LOUVER, REFER TO MECHANICAL DRAWINGS
- ALUMINUM SLIDING ENTRY DOOR W/ INSULATED GLAZING
- BEACON: GLASS CROWN: MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE. - REFER TO
- PAINTED TUBE STEEL CANOPY W/ STAINED WOOD TRELLIS
- DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOF
- DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOR
- OVERFLOW SCUPPER -BASIS OF DESIGN NESCO MFG INC. - MODEL # SCT914R -- REFER ALSO TO DETAIL 6J/A6.12
- **ENVELOPE ACCENT FINISH -REFER TO STYLE GUIDE**
- ENVELOPE ACCENT FINISH -REFER TO STYLE GUIDE
- KYNAR FINISH ALUMINUM COPING SYSTEM -- COLOR TO MATCH ADJACENT MATERIAL
- REVIEW ALL VIEWS AROUND PROPERTY BEFORE LOCATING ALL ROOF TOP MECHANICAL UNITS. VERIFY ALL EQUIPMENT IS ADEQUATELY SCREENED
- PTAC CONDENSATE DRAIN RISER IN EXTERIOR WALL. ALL STACKED PTAC UNITS TO BE CONNECTED TO A VERTICAL STACK PIPE WHICH SHOULD DRAIN TO NEAREST STORM WATER PIPE OR INLET. GC TO PROVIDE VENTS AND TRAPS AS REQUIRED FOR THE RISER (RISER SHOWN ON ELEVATION ARE TYPICAL AND SHOULD BE PROVIDED AT ALL PTAC UNITS)
- AINTED LINEN CHUTE, COLOR TO MATCH ADJACENT ENVELOPE
- DRIER EXHAUST VENT -- COLOR TO MATCH ADJACENT MATERIAL
- LIGHT FIXTURE --REFER TO CEILING PLAN
- EXPANSION JOINT @ FLOOR LINE AND WINDOW HEAD W/ BACKER ROD AND SEALAN' "LUMINUM LOUVER -- COLOR TO MATCH ADJACENT MATERIAL MATCHING ADJACEN
- THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION.
- NTERIOR CEILING HEIGHT DIMENSIONS ARE CRITICAL TO MAINTAIN AS MINIMUMS.
- ACCESS OPENING INTO BEACON
- ALUMINUM GUTTER AND DS. COLOR TO MATCH ADJ. MATERIAL

CONCRETE PAD - REFER TO AREA DEVELOPMENT PLAN

- ACCESSIBLE KEY CARD READER WIRED TO ELECTRIC STRIKE IN DOOR TOP OF
- POOL PERIMETER FENCE AS REQUIRED BY CODE W/ 3'-0" GATE WITH PANIC/EXIT
- PRECAST COPING

| READER AT 48" MAX. HEIGHT

- HI-LO DRINKING FOUNTAIN
- EMERGENCY PHONE
- ADA COMPLIANT STANDING SHOWER. PROVIDE TILE WALL AT SHOWER

| FINIS | SH KEY: | |
|-------|---------|--|
| | | |

- EX 1 ENVELOPE ACCENT METAL ALUMINUM COLOR WHITE EX 2 NOT USED EX 3 ENVELOPE - WRAP - EIFS, COLOR: BENJ. MOORE AC-22, NANTUCKET FOG EX 4 ENVELOPE - LINK & KEEP - EIFS, COLOR: BENJ. MOORE HC-70 VAN, BUREN
- EX 5 ENVELOPE ACCENT EIFS, COLOR: BENJ. MOORE 2111-50, STONE HARBOR
- X 6 ENVELOPE CORE EIFS, COLOR: BENJ. MOORE 2109-40, SMOKED OYSTER ENVELOPE - SUPPORT - CAST STONE. ARRISCRAFT RENAISSANCE -
- SERIES GINGER-SMOOTH FINISH ENVELOPE - SUPPORT - CAST STONE. ARRISCRAFT RENAISSANCE -SERIES GINGER-ROCKED FINISH BEAM: VERTICAL ELEMENT
- SET BACK IN PLANE FROM THE KEEP, EXTENDS TO THE GROUND. COMPOSED OF THREE ELEMENTS EX 9A REVEAL: VERTICAL ELEMENT EXTENDING THROUGH THE KEEP,
- EIFS, COLOR: BENJ. MOORE 398, FLOWER POWER EX 9B BAND: HORIZONTAL ELEMENT CAPPING THE REVEAL EIFS, COLOR: BENJ. MOORE 398, FLOWER POWER
- GLASS CROWN MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE.
- EX 10 ENVELOPE CANOPY STEEL, COLOR: BENJ. MOORE OC-17, WHITE DOVE EIFS TYPE/THICKNESS NOTE:
- INDICATES EIFS NUMBER WITH

 CORRESPONDING COLORS SHOWN ABOVE

 INDICATES EIFS THICKNESS. IF NO NUMBER IS

 SHOWN, THE EIFS IS 2" NOMINAL THICK

BUILDING SIGNAGE:

• ALL SIGNAGE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. LOCATION SIZE, ETC. TO BE DETERMINED ON AN INDIVIDUAL PROJECT BASIS. • MINIMUM 3/4" PLYWOOD BACKER-BOARD REQUIRED AT SIGN LOCATIONS. AREA SHOULD COVER ENTIRE LENGTH AND HEIGHT OF FASCIA OR SPACE AVAILABLE FOR SIGN. • ELECTRICAL AND FINAL CONNECTION BY CONTRACTOR. ELECTRICAL REQUIREMENTS MAY BE

OBTAINED FROM SIGN COMPANY. TYPICAL 120V REQUIREMENT. • RACEWAYS/ WIREWAYS ARE NOT ALLOWED. PERMANENT ACCESS DOORS TO INTERIOR OF ALL PARAPETS WHERE SIGNS ARE LOCATED TO B PROVIDED BY CONTRACTOR. CONTRACTOR TO FURNISH AND CONNECT PRIMARY ELECTRICAL. SERVICE INSIDE PARAPET WALL.

GUESTROOMS OR PUBLIC SPACES WITH SIGNAGE ON EXTERIOR WALLS MUST HAVE ELECTRICAL PENETRATIONS AND/ OR INSTALLED PRIOR TO CLOSING UP THE WALL. PENETRATION LAYOUT MA BE OBTAINED FROM SIGN COMPANY.

GENERAL NOTES

- REFER TO HOME 2 SUITES BY HILTON STANDARDS MANUAL GUIDE FOR ADDITIONAL INFORMATION REGARDING EXTERIOR MATERIALS, CONSTRUCTION REQUIREMENTS AND SIGNAGE REQUIREMENTS.
- GROUT COLOR TO MATCH EXTERIOR TILE OR MASONRY.
- BUILDING HEIGHT DIMENSIONS ARE BASED ON WOOD FRAME CONSTRUCTION. THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION. INTERIOR CEILING HEIGHTS ARE CRITICAL TO MAINTAIN AS MINIMUMS.
- EIFS FINISH SYSTEM OVER MIN. 2" NOMINAL THICK EPS ON EXTERIOR SHEATHING AND APPROVED BUILDING WRAP. APPROVED MANUFACTURER PER HILTON SPECS - "WATER MASTER DRAINABLE SYSTEM" OR EQUAL TO BE USED. SEE MANUFACTURER'S SPECS. FOR NSTALLATION DETAILS.



ILMMPHT ID #55106

WILMINGTON, NC. 19-088ND



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MR. MEHUL PATEL SUITE 101, 130 CANAL STREET, POOLER, GA 31322

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REVISION # DATE

| DATE | ISSUED TO | ISSUED FOR |
|------------|-----------|-------------------|
| 10-14-2019 | OWNER | FLOOR PLAN REVIEW |
| 01-23-2020 | HILTON | 25% REVIEW |
| 10-16-2020 | HILTON | 60% SUBMITTAL |

REMARKS

PROJECT ADDRESS: **HOME2 SUITES** BY HILTON

WILMINGTON, NORTH CAROLINA

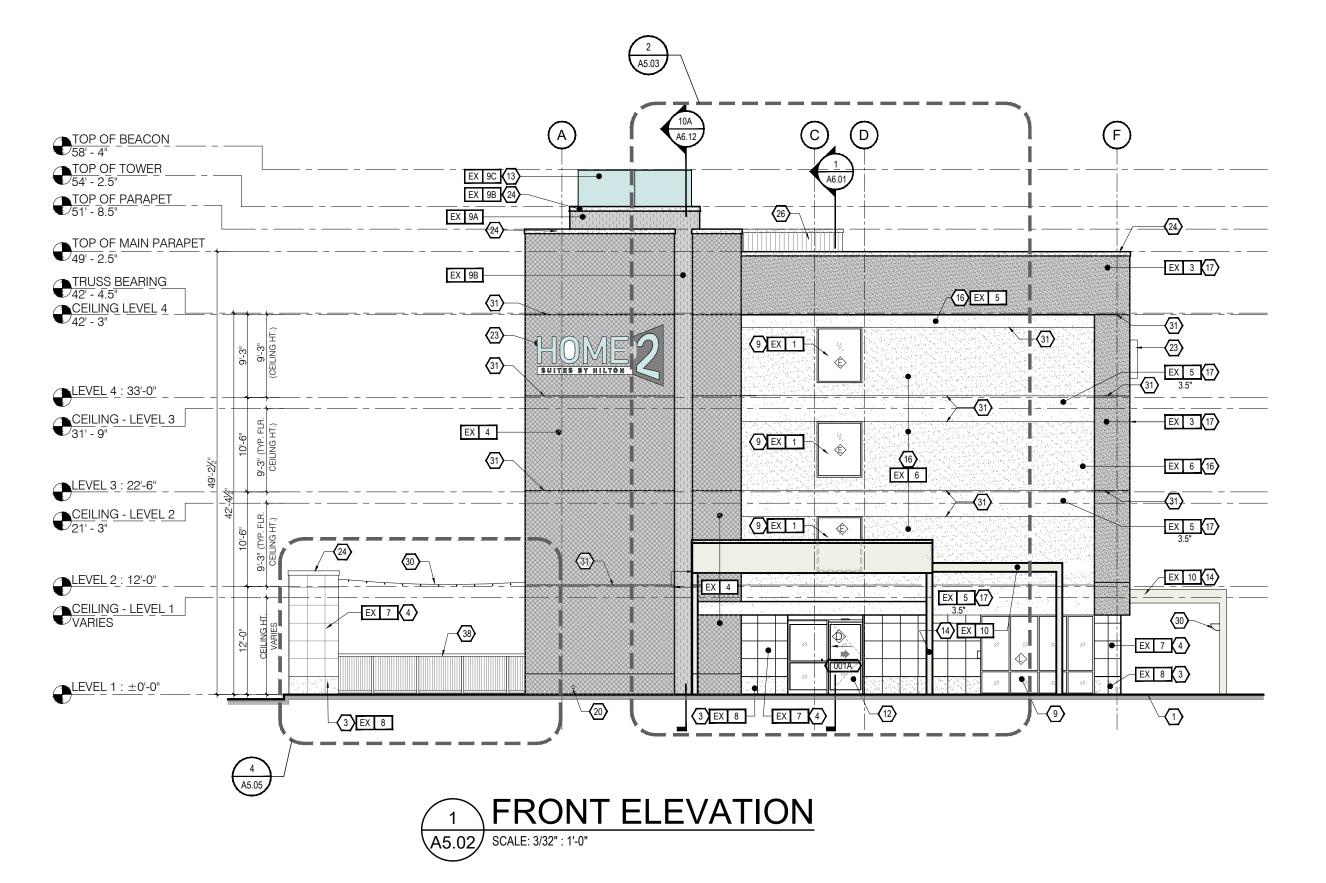
BARCLAY WEST TRACT B

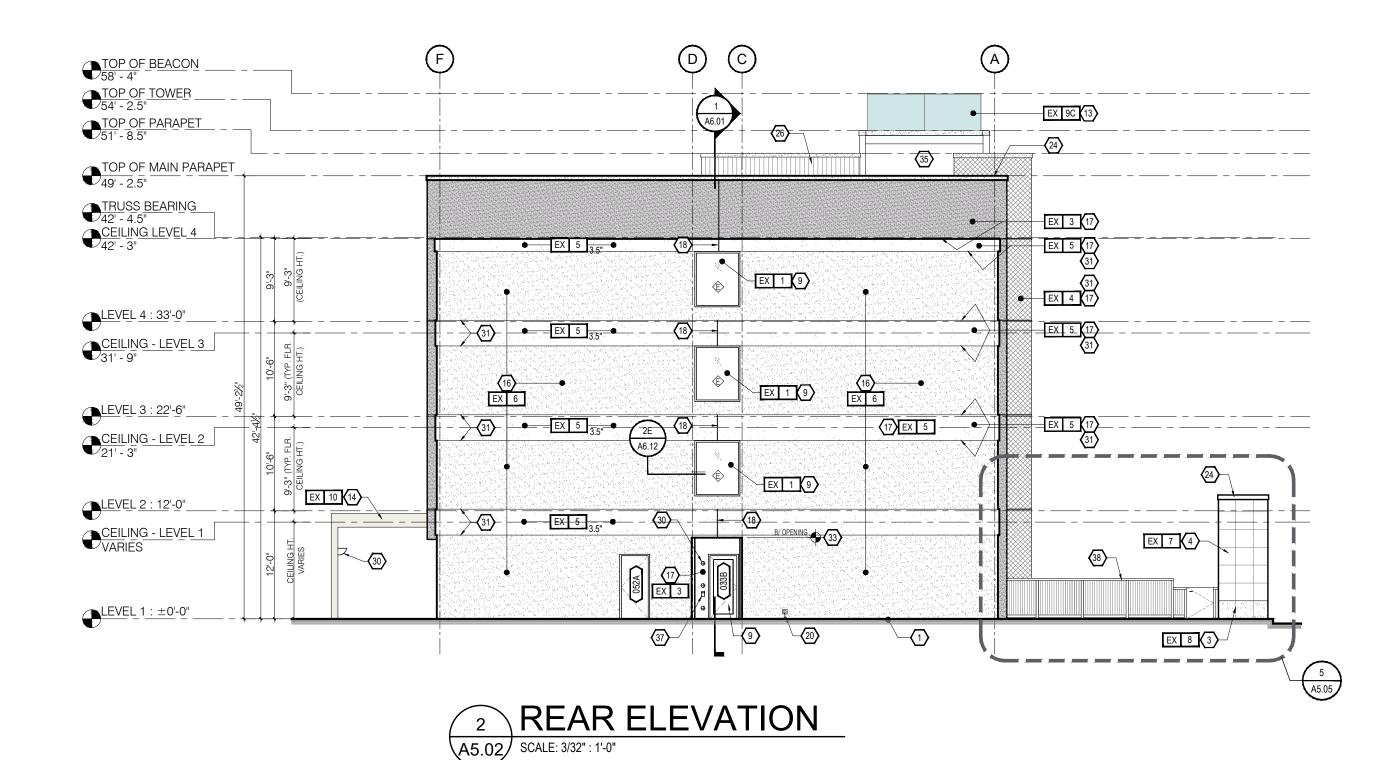
BUILDING ELEVATION

PAPER SIZE: 24X36 (ARCH D) SCALE: AS NOTED

SHEET NUMBER:

PROJECT NUMBER:





Approved Construction Plan Public Services • Engineering Division APPROVED STORMWATER MANAGEMENT PLAN

OVERLAP ALL HORIZONTAL AND VERTICAL SEAMS BY 6" MINIMUM. TAPE AND SEAL ALL SEAMS, BOTH VERTICAL AND HORIZONTAL WITH TAPE APPROVED BY AIR AND MOISTURE BARRIER MANUFACTURER

. PROVIDE SPRAY APPLIED AIR TIGHT FOAM INSULATING AIR SEAL ALL AROUND DOOR/WINDOW FRAMES. SEAL ALL GAPS AND PENETRATIONS FROM INSIDE AND OUTSIDE THE BUILDING PROVIDE CONT. FLASHING WITH BACKER ROD AND SEALANT AT WINDOW SILL AND LINTEL 5. FILL ALL AIR GAPS AT WINDOW JAMB WITH SPRAY APPLIED FOAM INSULATION

- STORM WATER MANHOLES MIN. 1.5" GAP SHOULD BE MAINTAINED BETWEEN ALL MASONRY AND EXTERIOR SHEATHING
- . ANY LUMBER IN CONTACT WITH CONCRETE WOULD BE PRESSURE TREATED (P.T.). PROVIDE FLASHING AND BACKER ROD WITH SEALANTS AS SHOWN AND AS REQUIRED. IF ANY PLACE WITH "NO" ADDITIONAL COST TO THE OWNER.
- GROUND MOUNTED CONDENSING UNITS SHOULD BE SCREENED WITH MATERIAL AS REQUIRED AND APPROVED BY HILTON. ALL UTILITY METERS (GAS, WATER AND ELECTRICAL METERS) SHOULD BE SCREENED IF REQUIRED
- 9. ALL LOUVERS AND WALL CAP SHOULD BE PAINTED TO MATCH ADJACENT WALL COLOR 10. ANY ITEMS/SYSTEM NOT SHOWN ON THE DRAWINGS THAT MAY BE REQUIRED DURING CONSTRUCTION WOULD BE PROVIDED BY THE CONTRACTOR WITHOUT ANY ADDITIONAL COST TO

REFER TO ALL OTHER SHEETS FOR ADDITIONAL NOTES AND INFORMATION.

- ALL WORK SHALL COMPLY TO THE INTERNATIONAL BUILDING CODE AND ALL ADOPTED SUB-CODES. DESIGN CONSULTANT AND ARCHITECTS ARE NOT RETAINED FOR ANY ON SITE
- SUPERVISION OR CONTRACT ADMINISTRATION. ALL WORK IS TO BE PERFORMED TO THE HIGHEST STANDARDS OF THE TRADES.
- IT SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO COORDINATE ALL WORK WITH THE STRUCTURAL, MECHANICAL, ELECTRICAL AND OTHER CONSULTANTS. THE GENERAL CONTRACTOR TO SUBMIT A NEW LAYOUT IF IT BECOMES NECESSARY FOR APPROVAL.
- THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH ALL EXISTING CONDITIONS AT THE JOB SITE PRIOR TO THE START OF ANY WORK. CONTRACTOR IS TO PROVIDE ACCESS PANELS AS REQUIRED NEEDED OR DIRECTED BY DESIGN
- . CONTRACTOR SHALL PROVIDE A FULL SCALE MOCKUP WALL WITH MATERIAL AND COLORS AS SPECIFIED IN THE DRAWINGS FOR THE APPROVAL OF OWNER BEFORE FINALIZING THE INSTALLATION OF SAID MATERIAL TO BUILDING.

AIR & MOISTURE BARRIER NOTES:

FOR FINISH FLOOR ELEVATIONS, REFER TO CIVIL PLANS AND DETAILS . ALL RAIN WATER PIPES SHOULD BE CONNECTED TO NEAREST STORM WATER SYSTEM PIPES OR

- . APPROVED EIFS SYSTEM PER HILTON SPECS. ALL EIFS SYSTEM TO BE INSTALLED WITH DRAINABLE
- DRAWING NOT SHOWN AND IS REQUIRED PER SITE CONDITIONS, CONTRACTOR WILL PROVIDE THEM
- BY CITY. USE MATERIAL AND FINISH AS APPROVED BY HILTON.

- THE CONTRACTOR SHALL NOTIFY THE DESIGN CONSULTANT, ARCHITECT AND ENGINEERS FOR INSTRUCTIONS / CLARIFICATIONS PRIOR TO CONSTRUCTION. CONTRACTOR IS TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS AT THE JOB SITE. DESIGN CONSULTANTS AND ARCHITECTS ARE TO BE NOTIFIED IN WRITING OF ANY
- CONTRACTOR IS TO COMPLY TO ALL STATE AND LOCAL ORDINANCES
- SUPPORT ALL WORK AS NEW WORK PROGRESSES
- CONSULTANTS, ARCHITECT, ENGINEER, CONSTRUCTION OFFICIAL OR OWNER. 0. CONTRACTOR AND/ OR OWNER SHALL PROVIDE FOR THE SAFETY OF ALL JOB RELATED PERSONNEL AS WELL AS THE GENERAL PUBLIC.

NOTES TO GC & ALL SUBS:
NO PRICING SHOULD BE DONE FROM A PARTIAL SET AND NO CHANGE ORDER WILL BE ALLOWED FOR

TRADES SHOULD CROSS REFERENCE EVERY TRADE FOR ADDITIONAL INFORMATION, CLARIFICATION

NO CHANGE ORDERS WILL BE ALLOWED DUE TO LACK OF COORDINATION AND NON REVIEW OF OTHER

THESE PLAN AND DETAILS ARE PREPARED USING THE REQUIRED GENERAL STANDARD OF CARE.

PERFECTION IS NOT POSSIBLE. WHEN ERRORS OR DISCREPANCIES ARE NOTED PLEASE NOTIFY THE

TRADES. IF ANY INFORMATION IS MISSING ON THE DRAWINGS, GC AND SUBS SHOULD CONTACT DESIGN

PRICING BASED ONLY ON A PARTIAL SET OR REVIEW OF ONLY A SINGLE TRADE'S DRAWINGS. ALL

ALL SUBS SHOULD CROSS REFERENCE AND REVIEW EACH AND EVERY TRADE INCLUDING CIVIL,

ARCHITECTURAL, STRUCTURAL AND MEP DRAWINGS FOR PRICING.

DESIGN TEAM FOR CLARIFICATION AS REQUIRED.

TEAM FOR CLARIFICATIONS BEFORE PROCEEDING WITH FURTHER WORK ON SITE

AND COORDINATION.

KEY NOTES:

- APPROXIMATE LINE OF GRADE
- DRIVEWAY AND DROP-OFF BELOW CANOPY
- MASONRY OR TILE BASE
- MASONRY OR TILE FIELD ASHLAR PATTERN
- NOT USED NOT USED
- ALUMINUM FIXED WINDOW W/ THERMAL BROKEN FRAME, INSULATED GLAZING WITH INTEGRAL ALUMINUM LOUVER AT PTAC UNITS. SEE SCHEDULE FOR GLASS TYPE.
- ALUMINUM STOREFRONT SYSTEM W/ THERMAL BROKEN FRAME AND INSULATED GLAZING. SEE SCHEDULE FOR GLASS TYPE.
- INSULATED HOLLOW CORE METAL DOOR. SEE DOOR WINDOW SCHEDULE
- MECHANICAL LOUVER, REFER TO MECHANICAL DRAWINGS
- ALUMINUM SLIDING ENTRY DOOR W/ INSULATED GLAZING
- BEACON: GLASS CROWN: MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE. - REFER TO SECTIONS FOR INFORMATION
- PAINTED TUBE STEEL CANOPY W/ STAINED WOOD TRELLIS
- DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOR
- DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOR
- FINISH CONTROL JOINT
- OVERFLOW SCUPPER -BASIS OF DESIGN NESCO MFG INC. - MODEL # SCT914R -- REFER ALSO TO DETAIL 6J/A6.12
- TAMPER RESISTANT, RECESSED HOSE BIB MOUNT TOP AT +12" A.F.F.
- ENVELOPE ACCENT FINISH -REFER TO STYLE GUIDE
- ENVELOPE ACCENT FINISH -REFER TO STYLE GUIDE
- SIGNAGE --REFER TO EXTERIOR SIGNAGE SPECIFICATIONS
- KYNAR FINISH ALUMINUM COPING SYSTEM --COLOR TO MATCH ADJACENT MATERIAL
- REVIEW ALL VIEWS AROUND PROPERTY BEFORE LOCATING ALL ROOF TOP MECHANICAL UNITS. VERIFY ALL EQUIPMENT IS ADEQUATELY SCREENED
- PTAC CONDENSATE DRAIN RISER IN EXTERIOR WALL. ALL STACKED PTAC UNITS TO BE CONNECTED TO A VERTICAL STACK PIPE WHICH SHOULD DRAIN TO NEAREST STORM WATER PIPE OR INLET. GC TO PROVIDE VENTS AND TRAPS AS REQUIRED FOR THE RISER (RISER SHOWN ON ELEVATION ARE TYPICAL AND SHOULD BE PROVIDED AT ALL PTAC UNITS)
- PAINTED LINEN CHUTE, COLOR TO MATCH ADJACENT ENVELOPE
- DRIER EXHAUST VENT -- COLOR TO MATCH ADJACENT MATERIAL
- LIGHT FIXTURE --REFER TO CEILING PLAN
- EXPANSION JOINT @ FLOOR LINE AND WINDOW HEAD W/ BACKER ROD AND SEALANT ALUMINUM LOUVER -- COLOR TO MATCH ADJACENT MATERIAL MATCHING ADJACENT
- BUILDING HEIGHT DIMENSIONS ARE BASED ON WOOD FRAME CONSTRUCTION. THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION.
- INTERIOR CEILING HEIGHT DIMENSIONS ARE CRITICAL TO MAINTAIN AS MINIMUMS.
- CONCRETE PAD REFER TO AREA DEVELOPMENT PLAN
- ACCESS OPENING INTO BEACON
- ALUMINUM GUTTER AND DS. COLOR TO MATCH ADJ. MATERIAL
- ACCESSIBLE KEY CARD READER WIRED TO ELECTRIC STRIKE IN DOOR TOP OF READER AT 48" MAX. HEIGHT
- POOL PERIMETER FENCE AS REQUIRED BY CODE W/ 3'-0" GATE WITH PANIC/EXIT
- HARDWARE
- PRECAST COPING
- HI-LO DRINKING FOUNTAIN
- EMERGENCY PHONE
- ADA COMPLIANT STANDING SHOWER. PROVIDE TILE WALL AT SHOWER

| FINIS | SH KEY: |
|-------|--|
| EX 1 | ENVELOPE - ACCENT METAL - ALUMINUM COLOR WHITE |
| EX 2 | NOT USED |
| EX 3 | ENVELOPE - WRAP - EIFS, COLOR: BENJ. MOORE AC-22, NANTUCKET FOG |
| EX 4 | ENVELOPE - LINK & KEEP - EIFS, COLOR: BENJ. MOORE HC-70 VAN, BUREN BROWN |
| | |

- EX 5 ENVELOPE ACCENT EIFS, COLOR: BENJ. MOORE 2111-50, STONE HARBOR EX 6 ENVELOPE - CORE - EIFS, COLOR: BENJ. MOORE 2109-40, SMOKED OYSTER
- ENVELOPE SUPPORT CAST STONE. ARRISCRAFT RENAISSANCE -SERIES GINGER-SMOOTH FINISH
- EX 8 ENVELOPE SUPPORT CAST STONE. ARRISCRAFT RENAISSANCE SERIES GINGER-ROCKED FINISH EX 9 BEAM: VERTICAL ELEMENT SET BACK IN PLANE FROM THE KEEP, EXTENDS TO THE GROUND. COMPOSED OF THREE ELEMENTS
- EX 9A REVEAL: VERTICAL ELEMENT EXTENDING THROUGH THE KEEP, EIFS, COLOR: BENJ. MOORE 398, FLOWER POWER EX 9B BAND: HORIZONTAL ELEMENT CAPPING THE REVEAL
- EIFS, COLOR: BENJ. MOORE 398, FLOWER POWER EX 9C GLASS CROWN MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR POLYMER, INTERNALLY LIT WITH MINIMAL,
- UNOBTRUSIVE STRUCTURE. EX | 10 | ENVELOPE - CANOPY - STEEL, COLOR: BENJ. MOORE OC-17, WHITE DOVE IFS TYPE/THICKNESS NOTE:
 INDICATES EIFS NUMBER WITH

INDICATES EIFS NUMBER WITH CORRESPONDING COLORS SHOWN ABOVE INDICATES EIFS THICKNESS. IF NO NUMBER IS SHOWN, THE EIFS IS 2" NOMINAL THICK

BUILDING SIGNAGE:

• ALL SIGNAGE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. LOCATION SIZE, ETC. TO BE DETERMINED ON AN INDIVIDUAL PROJECT BASIS.

• MINIMUM 3/4" PLYWOOD BACKER-BOARD REQUIRED AT SIGN LOCATIONS. AREA SHOULD COVER ENTIRE LENGTH AND HEIGHT OF FASCIA OR SPACE AVAILABLE FOR SIGN. • ELECTRICAL AND FINAL CONNECTION BY CONTRACTOR. ELECTRICAL REQUIREMENTS MAY BE OBTAINED FROM SIGN COMPANY. TYPICAL 120V REQUIREMENT.

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GUESTROOMS OR PUBLIC SPACES WITH SIGNAGE ON EXTERIOR WALLS MUST HAVE ELECTRICAL PENETRATIONS AND/ OR INSTALLED PRIOR TO CLOSING UP THE WALL. PENETRATION LAYOUT MA BE OBTAINED FROM SIGN COMPANY.

GENERAL NOTES

- R TO HOME 2 SUITES BY HILTON STANDARDS MANUAL GUIDE FOR ADDITIONAL MATERIALS, CONSTRUCTION REQUIREMENTS AND
- GROUT COLOR TO MATCH EXTERIOR TILE OR MASONRY.
- BUILDING HEIGHT DIMENSIONS ARE BASED ON WOOD FRAME CONSTRUCTION. THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION. INTERIOR CEILING HEIGHTS ARE CRITICAL TO MAINTAIN AS MINIMUMS.
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WILMINGTON, NC. 19-088ND ILMMPHT ID #55106

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REMARKS

REVISION # DATE

| DATE | ISSUED TO | ISSUED FOR |
|--|-----------------|------------------------------|
| | ISSUED TO | ISSUED FOR FLOOR PLAN REVIEW |
| 10-14-2019 | | |
| DATE 10-14-2019 01-23-2020 10-16-2020 | OWNER | FLOOR PLAN REVIEW |
| 10-14-2019 01-23-2020 | OWNER HILTON | FLOOR PLAN REVIEW 25% REVIEW |

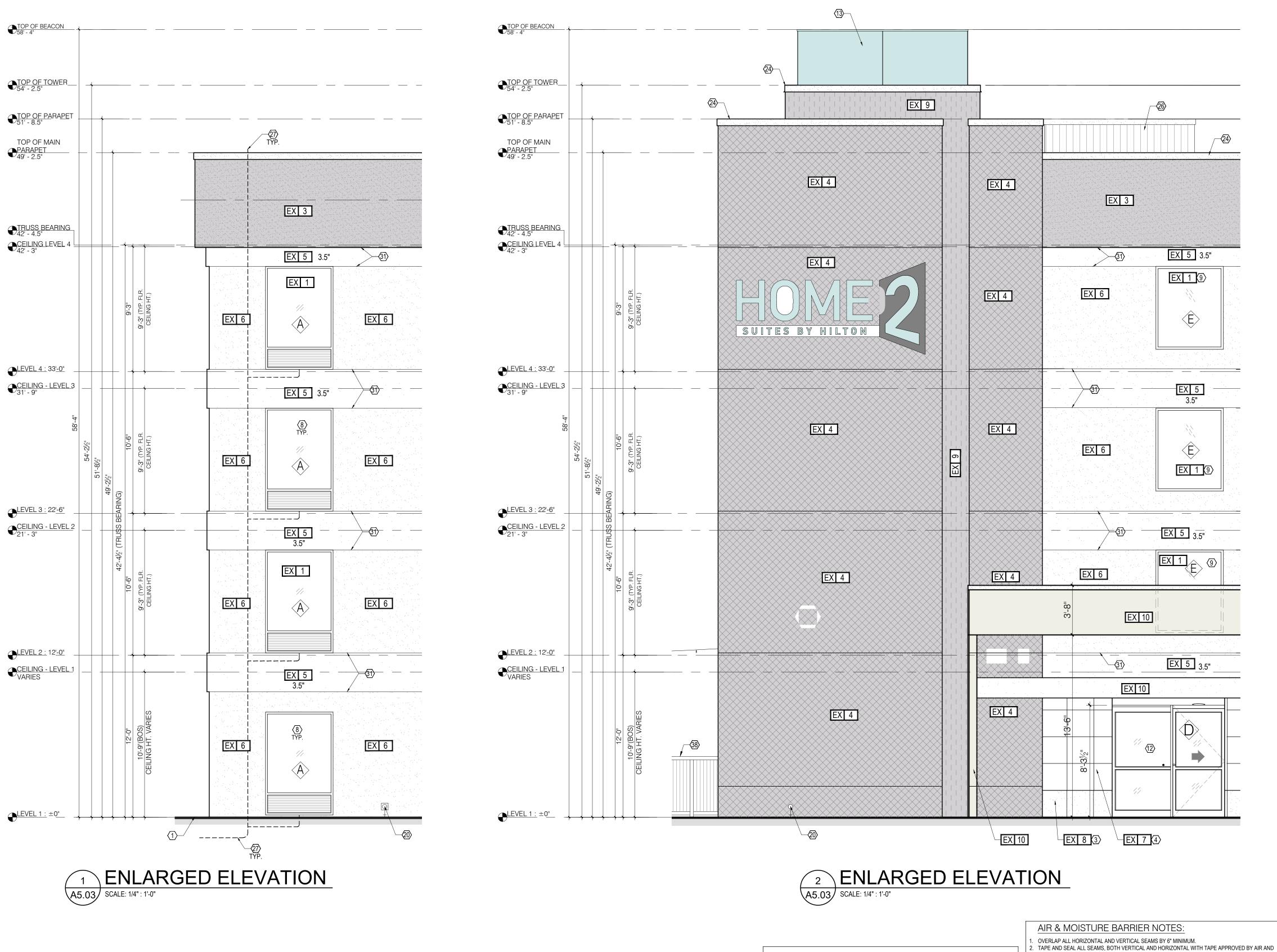
PROJECT ADDRESS: **HOME2 SUITES** BY HILTON **BARCLAY WEST TRACT B** WILMINGTON, NORTH CAROLINA

BUILDING ELEVATION

PAPER SIZE: 24X36 (ARCH D) SCALE: AS NOTED

SHEET NUMBER

PROJECT NUMBER:



NOTES TO GC & ALL SUBS:

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ALL SUBS SHOULD CROSS REFERENCE AND REVIEW EACH AND EVERY TRADE INCLUDING CIVIL, ARCHITECTURAL, STRUCTURAL AND MEP DRAWINGS FOR PRICING.

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| Approved Construction Plan | IV/CITY OF |
|----------------------------|--|
| <u>Name</u> Date | WILMINGTON |
| Planning | NORTH CAROLINA |
| | Public Services Engineering Division |
| Traffic | — APPROVED STORMWATER MANAGEMENT PLAN |
| Fire | — Date: Permit # |
| | Signed: |

MOISTURE BARRIER MANUFACTURER.

PROVIDE SPRAY APPLIED AIR TIGHT FOAM INSULATING AIR SEAL ALL AROUND DOOR/WINDOW FRAMES. SEAL ALL GAPS AND PENETRATIONS FROM INSIDE AND OUTSIDE THE BUILDING 4. PROVIDE CONT. FLASHING WITH BACKER ROD AND SEALANT AT WINDOW SILL AND LINTEL 5. FILL ALL AIR GAPS AT WINDOW JAMB WITH SPRAY APPLIED FOAM INSULATION

GENERAL NOTES - ELEVATION

HIGHEST STANDARDS OF THE TRADES.

SITE PRIOR TO THE START OF ANY WORK.

PERSONNEL AS WELL AS THE GENERAL PUBLIC.

INSTALLATION OF SAID MATERIAL TO BUILDING.

SUPPORT ALL WORK AS NEW WORK PROGRESSES.

DISCREPANCIES.

REFER TO ALL OTHER SHEETS FOR ADDITIONAL NOTES AND INFORMATION.

INSTRUCTIONS / CLARIFICATIONS PRIOR TO CONSTRUCTION.

CONTRACTOR IS TO COMPLY TO ALL STATE AND LOCAL ORDINANCES.

THE CONTRACTOR SHALL NOTIFY THE DESIGN CONSULTANT, ARCHITECT AND ENGINEERS FOR

CONTRACTOR IS TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS AT THE JOB SITE.

DESIGN CONSULTANTS AND ARCHITECTS ARE TO BE NOTIFIED IN WRITING OF ANY

ALL WORK SHALL COMPLY TO THE INTERNATIONAL BUILDING CODE AND ALL ADOPTED

SUB-CODES. DESIGN CONSULTANT AND ARCHITECTS ARE NOT RETAINED FOR ANY ON SITE

SUPERVISION OR CONTRACT ADMINISTRATION. ALL WORK IS TO BE PERFORMED TO THE

CONTRACTOR TO SUBMIT A NEW LAYOUT IF IT BECOMES NECESSARY FOR APPROVAL.

CONTRACTOR AND/ OR OWNER SHALL PROVIDE FOR THE SAFETY OF ALL JOB RELATED

SPECIFIED IN THE DRAWINGS FOR THE APPROVAL OF OWNER BEFORE FINALIZING THE

CONSULTANTS, ARCHITECT, ENGINEER, CONSTRUCTION OFFICIAL OR OWNER.

THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH ALL EXISTING CONDITIONS AT THE JOB

CONTRACTOR SHALL PROVIDE A FULL SCALE MOCKUP WALL WITH MATERIAL AND COLORS AS

- FOR FINISH FLOOR ELEVATIONS, REFER TO CIVIL PLANS AND DETAILS . ALL RAIN WATER PIPES SHOULD BE CONNECTED TO NEAREST STORM WATER SYSTEM PIPES OR STORM WATER MANHOLES
- MIN. 1.5" GAP SHOULD BE MAINTAINED BETWEEN ALL MASONRY AND EXTERIOR SHEATHING
- APPROVED EIFS SYSTEM PER HILTON SPECS. ALL EIFS SYSTEM TO BE INSTALLED WITH DRAINABLE
- IT SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO COORDINATE ALL WORK WITH THE STRUCTURAL, MECHANICAL, ELECTRICAL AND OTHER CONSULTANTS. THE GENERAL . ANY LUMBER IN CONTACT WITH CONCRETE WOULD BE PRESSURE TREATED (P.T.).
 - PROVIDE FLASHING AND BACKER ROD WITH SEALANTS AS SHOWN AND AS REQUIRED. IF ANY PLACE DRAWING NOT SHOWN AND IS REQUIRED PER SITE CONDITIONS, CONTRACTOR WILL PROVIDE THEM
- WITH "NO" ADDITIONAL COST TO THE OWNER. GROUND MOUNTED CONDENSING UNITS SHOULD BE SCREENED WITH MATERIAL AS REQUIRED AND CONTRACTOR IS TO PROVIDE ACCESS PANELS AS REQUIRED NEEDED OR DIRECTED BY DESIGN APPROVED BY HILTON.
 - ALL UTILITY METERS (GAS, WATER AND ELECTRICAL METERS) SHOULD BE SCREENED IF REQUIRED BY CITY. USE MATERIAL AND FINISH AS APPROVED BY HILTON.
 - . ALL LOUVERS AND WALL CAP SHOULD BE PAINTED TO MATCH ADJACENT WALL COLOR 10. ANY ITEMS/SYSTEM NOT SHOWN ON THE DRAWINGS THAT MAY BE REQUIRED DURING CONSTRUCTION WOULD BE PROVIDED BY THE CONTRACTOR WITHOUT ANY ADDITIONAL COST TO THE OWNER.

KEY NOTES:

- APPROXIMATE LINE OF GRADE
- DRIVEWAY AND DROP-OFF BELOW CANOPY
- MASONRY OR TILE BASE
- MASONRY OR TILE FIELD ASHLAR PATTERN

- ALUMINUM FIXED WINDOW W/ THERMAL BROKEN FRAME, INSULATED GLAZING WITH INTEGRAL ALUMINUM LOUVER AT PTAC UNITS. SEE SCHEDULE FOR GLASS TYPE.
- ALUMINUM STOREFRONT SYSTEM W/ THERMAL BROKEN FRAME AND INSULATED GLAZING. SEE SCHEDULE FOR GLASS TYPE.
- INSULATED HOLLOW CORE METAL DOOR. SEE DOOR WINDOW SCHEDULE MECHANICAL LOUVER, REFER TO MECHANICAL DRAWINGS
- ALUMINUM SLIDING ENTRY DOOR W/ INSULATED GLAZING
- BEACON: GLASS CROWN: MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR
- POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE. REFER TO SECTIONS FOR INFORMATION
- PAINTED TUBE STEEL CANOPY W/ STAINED WOOD TRELLIS
- DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOR
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- FINISH CONTROL JOINT
- OVERFLOW SCUPPER -BASIS OF DESIGN NESCO MFG INC. - MODEL # SCT914R -- REFER ALSO TO DETAIL 6J/A6.12
- TAMPER RESISTANT, RECESSED HOSE BIB MOUNT TOP AT +12" A.F.F.
- ENVELOPE ACCENT FINISH -REFER TO STYLE GUIDE
- ENVELOPE ACCENT FINISH -REFER TO STYLE GUIDE
- SIGNAGE --REFER TO EXTERIOR SIGNAGE SPECIFICATIONS
- KYNAR FINISH ALUMINUM COPING SYSTEM --COLOR TO MATCH ADJACENT MATERIAL
- REVIEW ALL VIEWS AROUND PROPERTY BEFORE LOCATING ALL ROOF TOP MECHANICAL UNITS. VERIFY ALL EQUIPMENT IS ADEQUATELY SCREENED
- PTAC CONDENSATE DRAIN RISER IN EXTERIOR WALL. ALL STACKED PTAC UNITS TO BE CONNECTED TO A VERTICAL STACK PIPE WHICH SHOULD DRAIN TO NEAREST STORM WATER PIPE OR INLET. GC TO PROVIDE VENTS AND TRAPS AS REQUIRED FOR THE RISER (RISER SHOWN ON ELEVATION ARE TYPICAL AND SHOULD BE PROVIDED AT ALL PTAC UNITS)
- PAINTED LINEN CHUTE, COLOR TO MATCH ADJACENT ENVELOPE
- DRIER EXHAUST VENT -- COLOR TO MATCH ADJACENT MATERIAL
- LIGHT FIXTURE --REFER TO CEILING PLAN
- EXPANSION JOINT @ FLOOR LINE AND WINDOW HEAD W/ BACKER ROD AND SEALANT ALUMINUM LOUVER -- COLOR TO MATCH ADJACENT MATERIAL MATCHING ADJACENT
- BUILDING HEIGHT DIMENSIONS ARE BASED ON WOOD FRAME CONSTRUCTION. THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION.
- INTERIOR CEILING HEIGHT DIMENSIONS ARE CRITICAL TO MAINTAIN AS MINIMUMS.
- CONCRETE PAD REFER TO AREA DEVELOPMENT PLAN
- ACCESS OPENING INTO BEACON
- ALUMINUM GUTTER AND DS. COLOR TO MATCH ADJ. MATERIAL
- ACCESSIBLE KEY CARD READER WIRED TO ELECTRIC STRIKE IN DOOR TOP OF READER AT 48" MAX. HEIGHT
- POOL PERIMETER FENCE AS REQUIRED BY CODE W/ 3'-0" GATE WITH PANIC/EXIT HARDWARE
- PRECAST COPING
- HI-LO DRINKING FOUNTAIN
- EMERGENCY PHONE
- ADA COMPLIANT STANDING SHOWER. PROVIDE TILE WALL AT SHOWER

| FII | NISI | H KEY: | | | | | |
|-----|------|------------------|--------------|---------|------------|------|--|
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| EX | 1 E | ENVELOPE - ACCEN | IT METAL - A | LUMINUI | M COLOR WH | HITE | |
| EX | 2 1 | IOT USED | | | | | |

- EX 3 ENVELOPE WRAP EIFS, COLOR: BENJ. MOORE AC-22, NANTUCKET FOG ENVELOPE - LINK & KEEP - EIFS, COLOR: BENJ. MOORE HC-70 VAN, BUREN
- EX 5 ENVELOPE ACCENT EIFS, COLOR: BENJ. MOORE 2111-50, STONE HARBOR EX 6 ENVELOPE - CORE - EIFS, COLOR: BENJ. MOORE 2109-40, SMOKED OYSTER
- EX 7 ENVELOPE SUPPORT CAST STONE. ARRISCRAFT RENAISSANCE SERIES GINGER-SMOOTH FINISH
- EX 8 ENVELOPE SUPPORT CAST STONE. ARRISCRAFT RENAISSANCE SERIES GINGER-ROCKED FINISH EX 9 BEAM: VERTICAL ELEMENT
- SET BACK IN PLANE FROM THE KEEP, EXTENDS TO THE GROUND. COMPOSED OF THREE ELEMENTS
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- EX 9C GLASS CROWN MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE.
- EX 10 ENVELOPE CANOPY STEEL, COLOR: BENJ. MOORE OC-17, WHITE DOVE

EIFS TYPE/THICKNESS NOTE: INDICATES EIFS NUMBER WITH CORRESPONDING COLORS SHOWN ABOVE INDICATES EIFS THICKNESS. IF NO NUMBER IS SHOWN, THE EIFS IS 2" NOMINAL THICK

BUILDING SIGNAGE:

• ALL SIGNAGE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. LOCATION SIZE, ETC. TO BE DETERMINED ON AN INDIVIDUAL PROJECT BASIS. • MINIMUM 3/4" PLYWOOD BACKER-BOARD REQUIRED AT SIGN LOCATIONS. AREA SHOULD COVER ENTIRE LENGTH AND HEIGHT OF FASCIA OR SPACE AVAILABLE FOR SIGN. • ELECTRICAL AND FINAL CONNECTION BY CONTRACTOR. ELECTRICAL REQUIREMENTS MAY BE OBTAINED FROM SIGN COMPANY. TYPICAL 120V REQUIREMENT.

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GUESTROOMS OR PUBLIC SPACES WITH SIGNAGE ON EXTERIOR WALLS MUST HAVE ELECTRICAL PENETRATIONS AND/ OR INSTALLED PRIOR TO CLOSING UP THE WALL. PENETRATION LAYOUT MA BE OBTAINED FROM SIGN COMPANY.

GENERAL NOTES

- REFER TO HOME 2 SUITES BY HILTON STANDARDS MANUAL GUIDE FOR ADDITIONAL INFORMATION REGARDING EXTERIOR MATERIALS, CONSTRUCTION REQUIREMENTS AND SIGNAGE REQUIREMENTS.
- GROUT COLOR TO MATCH EXTERIOR TILE OR MASONRY.
- BUILDING HEIGHT DIMENSIONS ARE BASED ON WOOD FRAME CONSTRUCTION. THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION. INTERIOR CEILING HEIGHTS ARE CRITICAL TO MAINTAIN AS MINIMUMS.
- EIFS FINISH SYSTEM OVER MIN. 2" NOMINAL THICK EPS ON EXTERIOR SHEATHING AND APPROVED BUILDING WRAP. APPROVED MANUFACTURER PER HILTON SPECS - "WATER MASTER DRAINABLE SYSTEM" OR EQUAL TO BE USED. SEE MANUFACTURER'S SPECS. FOR INSTALLATION DETAILS.



WILMINGTON, NC. 19-088ND ILMMPHT ID #55106

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OWNER

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REVISION # DATE REMARKS

| DATE | ISSUED TO | ISSUED FOR |
|------------|-----------|-------------------|
| 10-14-2019 | OWNER | FLOOR PLAN REVIEW |
| 01-23-2020 | HILTON | 25% REVIEW |
| 10-16-2020 | HILTON | 60% SUBMITTAL |

PROJECT ADDRESS: **HOME2 SUITES** BY HILTON BARCLAY WEST TRACT B

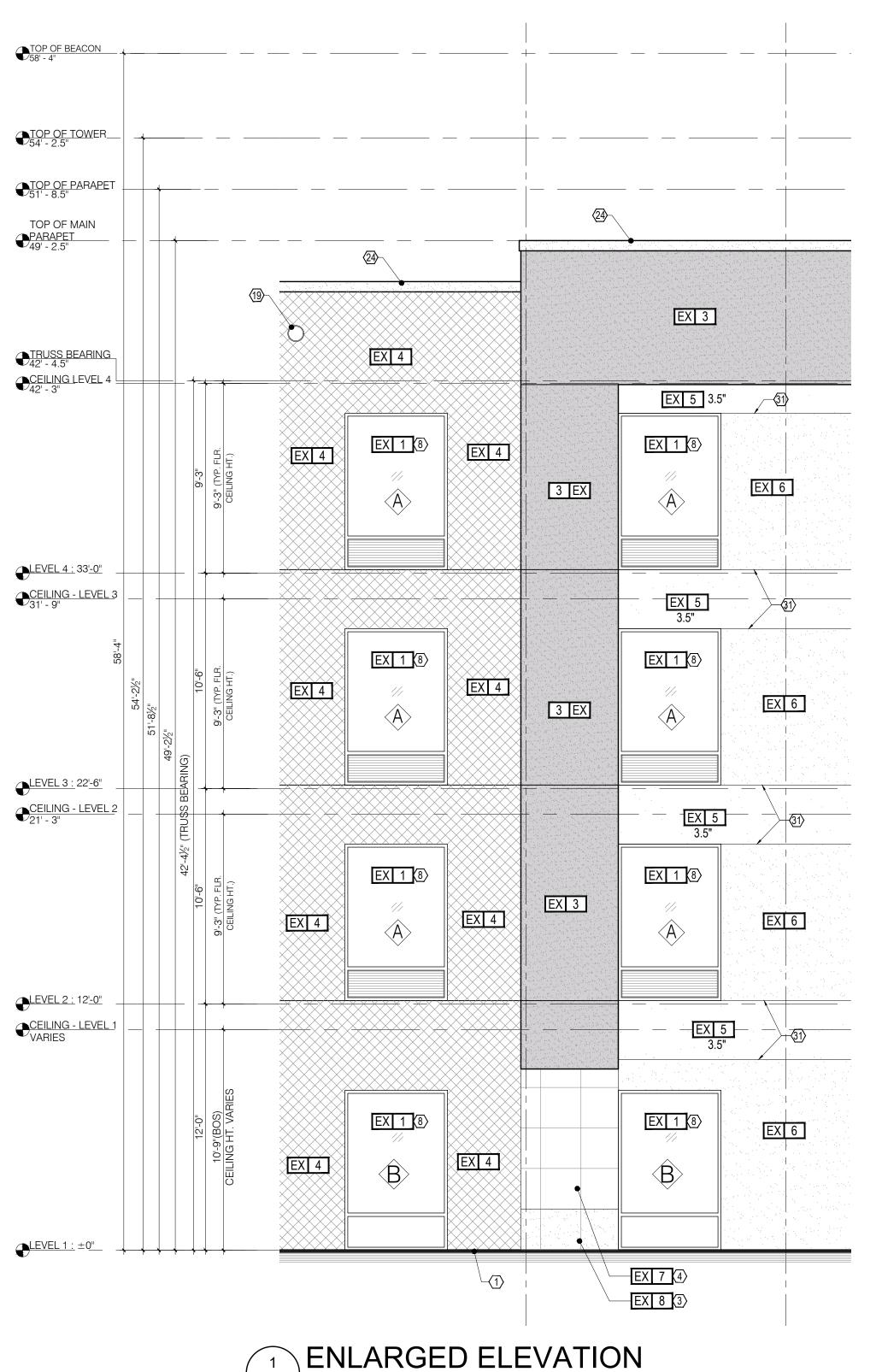
| WILMINGTON, NORTH CAROLINA

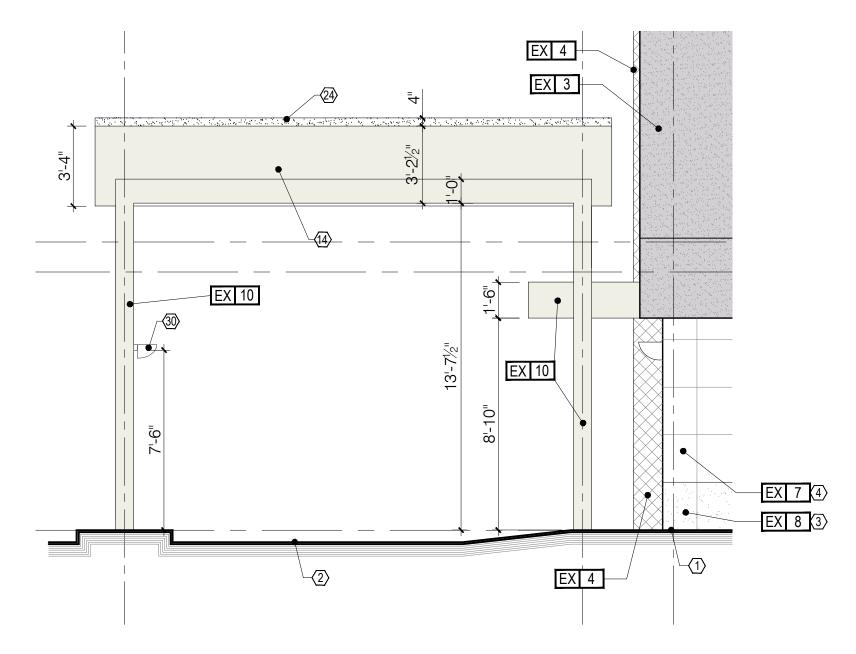
ENLARGED ELEVATION

PAPER SIZE: 24X36 (ARCH D)

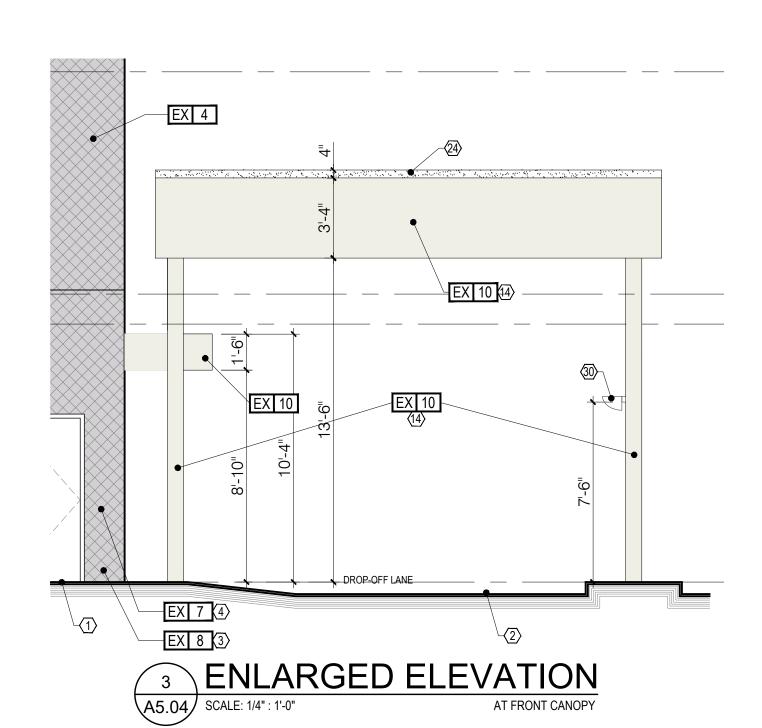
SCALE: AS NOTED PROJECT NUMBER:

SHEET NUMBER:





ENLARGED ELEVATION A5.04 | SCALE: 1/4" : 1'-0"



Approved Construction Plan Public Services • Engineering Division APPROVED STORMWATER MANAGEMENT PLAN

NOTES TO GC & ALL SUBS:

NO PRICING SHOULD BE DONE FROM A PARTIAL SET AND NO CHANGE ORDER WILL BE ALLOWED FOR PRICING BASED ONLY ON A PARTIAL SET OR REVIEW OF ONLY A SINGLE TRADE'S DRAWINGS. ALL TRADES SHOULD CROSS REFERENCE EVERY TRADE FOR ADDITIONAL INFORMATION, CLARIFICATION AND COORDINATION.

(A5.04) SCALE: 1/4" : 1'-0"

ALL SUBS SHOULD CROSS REFERENCE AND REVIEW EACH AND EVERY TRADE INCLUDING CIVIL, ARCHITECTURAL, STRUCTURAL AND MEP DRAWINGS FOR PRICING.

NO CHANGE ORDERS WILL BE ALLOWED DUE TO LACK OF COORDINATION AND NON REVIEW OF OTHER

TRADES. IF ANY INFORMATION IS MISSING ON THE DRAWINGS, GC AND SUBS SHOULD CONTACT DESIGN

TEAM FOR CLARIFICATIONS BEFORE PROCEEDING WITH FURTHER WORK ON SITE

THESE PLAN AND DETAILS ARE PREPARED USING THE REQUIRED GENERAL STANDARD OF CARE. PERFECTION IS NOT POSSIBLE. WHEN ERRORS OR DISCREPANCIES ARE NOTED PLEASE NOTIFY THE DESIGN TEAM FOR CLARIFICATION AS REQUIRED.

REFER TO ALL OTHER SHEETS FOR ADDITIONAL NOTES AND INFORMATION.

- THE CONTRACTOR SHALL NOTIFY THE DESIGN CONSULTANT, ARCHITECT AND ENGINEERS FOR INSTRUCTIONS / CLARIFICATIONS PRIOR TO CONSTRUCTION. CONTRACTOR IS TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS AT THE JOB SITE. DESIGN CONSULTANTS AND ARCHITECTS ARE TO BE NOTIFIED IN WRITING OF ANY
- ALL WORK SHALL COMPLY TO THE INTERNATIONAL BUILDING CODE AND ALL ADOPTED SUB-CODES. DESIGN CONSULTANT AND ARCHITECTS ARE NOT RETAINED FOR ANY ON SITE
- SUPERVISION OR CONTRACT ADMINISTRATION. ALL WORK IS TO BE PERFORMED TO THE HIGHEST STANDARDS OF THE TRADES. CONTRACTOR IS TO COMPLY TO ALL STATE AND LOCAL ORDINANCES.
- IT SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO COORDINATE ALL WORK WITH THE STRUCTURAL, MECHANICAL, ELECTRICAL AND OTHER CONSULTANTS. THE GENERAL CONTRACTOR TO SUBMIT A NEW LAYOUT IF IT BECOMES NECESSARY FOR APPROVAL.
- SUPPORT ALL WORK AS NEW WORK PROGRESSES.
- CONTRACTOR AND/ OR OWNER SHALL PROVIDE FOR THE SAFETY OF ALL JOB RELATED PERSONNEL AS WELL AS THE GENERAL PUBLIC. CONTRACTOR SHALL PROVIDE A FULL SCALE MOCKUP WALL WITH MATERIAL AND COLORS AS SPECIFIED IN THE DRAWINGS FOR THE APPROVAL OF OWNER BEFORE FINALIZING THE INSTALLATION OF SAID MATERIAL TO BUILDING.
- THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH ALL EXISTING CONDITIONS AT THE JOB SITE PRIOR TO THE START OF ANY WORK. CONTRACTOR IS TO PROVIDE ACCESS PANELS AS REQUIRED NEEDED OR DIRECTED BY DESIGN CONSULTANTS, ARCHITECT, ENGINEER, CONSTRUCTION OFFICIAL OR OWNER.

AIR & MOISTURE BARRIER NOTES OVERLAP ALL HORIZONTAL AND VERTICAL SEAMS BY 6" MINIMUM.

- TAPE AND SEAL ALL SEAMS, BOTH VERTICAL AND HORIZONTAL WITH TAPE APPROVED BY AIR AND MOISTURE BARRIER MANUFACTURER.
- PROVIDE SPRAY APPLIED AIR TIGHT FOAM INSULATING AIR SEAL ALL AROUND DOOR/WINDOW
- FRAMES. SEAL ALL GAPS AND PENETRATIONS FROM INSIDE AND OUTSIDE THE BUILDING
- . PROVIDE CONT. FLASHING WITH BACKER ROD AND SEALANT AT WINDOW SILL AND LINTEL . FILL ALL AIR GAPS AT WINDOW JAMB WITH SPRAY APPLIED FOAM INSULATION

- FOR FINISH FLOOR ELEVATIONS, REFER TO CIVIL PLANS AND DETAILS . ALL RAIN WATER PIPES SHOULD BE CONNECTED TO NEAREST STORM WATER SYSTEM PIPES OR
- STORM WATER MANHOLES MIN. 1.5" GAP SHOULD BE MAINTAINED BETWEEN ALL MASONRY AND EXTERIOR SHEATHING
- . APPROVED EIFS SYSTEM PER HILTON SPECS. ALL EIFS SYSTEM TO BE INSTALLED WITH DRAINABLE
- . ANY LUMBER IN CONTACT WITH CONCRETE WOULD BE PRESSURE TREATED (P.T.). . PROVIDE FLASHING AND BACKER ROD WITH SEALANTS AS SHOWN AND AS REQUIRED. IF ANY PLACE
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- 10. ANY ITEMS/SYSTEM NOT SHOWN ON THE DRAWINGS THAT MAY BE REQUIRED DURING CONSTRUCTION WOULD BE PROVIDED BY THE CONTRACTOR WITHOUT ANY ADDITIONAL COST TO THE OWNER.

- KEY NOTES:
- APPROXIMATE LINE OF GRADE
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- NOT USED NOT USED
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- ALUMINUM STOREFRONT SYSTEM W/ THERMAL BROKEN FRAME AND INSULATED
- GLAZING. SEE SCHEDULE FOR GLASS TYPE. INSULATED HOLLOW CORE METAL DOOR. SEE DOOR WINDOW SCHEDULE
- MECHANICAL LOUVER, REFER TO MECHANICAL DRAWINGS
- ALUMINUM SLIDING ENTRY DOOR W/ INSULATED GLAZING
- BEACON: GLASS CROWN: MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE. - REFER TO SECTIONS FOR INFORMATION
- PAINTED TUBE STEEL CANOPY W/ STAINED WOOD TRELLIS
- DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOR
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| FI | NISH KEY: | |
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- X 1 ENVELOPE ACCENT METAL ALUMINUM COLOR WHITE EX 2 NOT USED EX 3 ENVELOPE - WRAP - EIFS, COLOR: BENJ. MOORE AC-22, NANTUCKET FOG EX 4 ENVELOPE - LINK & KEEP - EIFS, COLOR: BENJ. MOORE HC-70 VAN, BUREN
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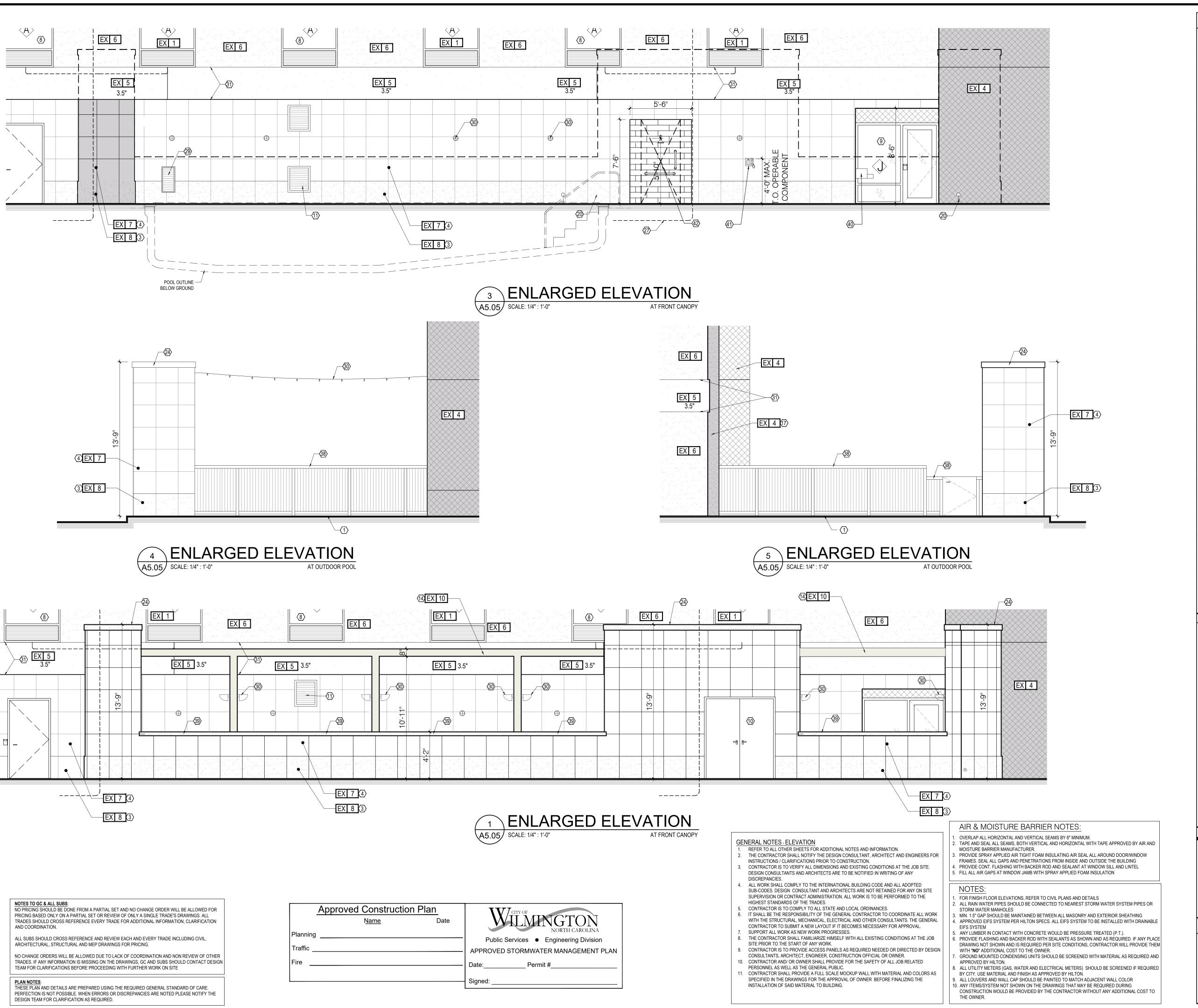
| DATE | ISSUED TO | ISSUED FOR |
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| 10-14-2019 | OWNER | FLOOR PLAN REVIEW |
| 01-23-2020 | HILTON | 25% REVIEW |
| 10-16-2020 | HILTON | 60% SUBMITTAL |
| | | |

PROJECT ADDRESS: **HOME2 SUITES** BY HILTON BARCLAY WEST TRACT B WILMINGTON, NORTH CAROLINA

ENLARGED ELEVATION

PAPER SIZE: 24X36 (ARCH D)

PROJECT NUMBER:



APPROXIMATE LINE OF GRADE DRIVEWAY AND DROP-OFF BELOW CANOPY MASONRY OR TILE - BASE MASONRY OR TILE - FIELD - ASHLAR PATTERN

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OWNER

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DATE ISSUED TO ISSUED FOR 10-14-2019 OWNER FLOOR PLAN REVIEW

01-23-2020 HILTON 25% REVIEW

10-16-2020 HILTON 60% SUBMITTAL

REVISION # DATE

PROJECT ADDRESS:

BY HILTON

HOME2 SUITES

4360 Chamblee Dunwoody Road, Suite 210

MECHANICAL UNITS. VERIFY ALL EQUIPMENT IS ADEQUATELY SCREENED PTAC CONDENSATE DRAIN RISER IN EXTERIOR WALL. ALL STACKED PTAC UNITS TO

BE CONNECTED TO A VERTICAL STACK PIPE WHICH SHOULD DRAIN TO NEAREST STORM WATER PIPE OR INLET, GC TO PROVIDE VENTS AND TRAPS AS REQUIRED. FOR THE RISER (RISER SHOWN ON ELEVATION ARE TYPICAL AND SHOULD BE PROVIDED AT ALL PTAC UNITS)

REVIEW ALL VIEWS AROUND PROPERTY BEFORE LOCATING ALL ROOF TOP

KYNAR FINISH ALUMINUM COPING SYSTEM --COLOR TO MATCH ADJACENT MATERIAL

ALUMINUM FIXED WINDOW W/ THERMAL BROKEN FRAME, INSULATED GLAZING WITH INTEGRAL ALUMINUM LOUVER AT PTAC UNITS. SEE SCHEDULE FOR GLASS TYPE

ALUMINUM STOREFRONT SYSTEM W/ THERMAL BROKEN FRAME AND INSULATED

BEACON: GLASS CROWN: MONOLITHIC ELEMENT RESTING ON THE BAND. INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR

POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE. - REFER TO

DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR

GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOR DRAINABLE EIFS FINISH ON EPS FOAM OVER WEATHER BARRIER AND EXTERIOR

GRADE SHEATHING. SEE WALL SCHEDULE. SEE EXTERIOR FINISH INDEX FOR COLOF

INSULATED HOLLOW CORE METAL DOOR. SEE DOOR WINDOW SCHEDULE

MECHANICAL LOUVER, REFER TO MECHANICAL DRAWINGS

ALUMINUM SLIDING ENTRY DOOR W/ INSULATED GLAZING

PAINTED TUBE STEEL CANOPY W/ STAINED WOOD TRELLIS

OVERFLOW SCUPPER -BASIS OF DESIGN NESCO MFG INC. - MODEL # SCT914R -- REFER ALSO TO DETAIL 6J/A6.12

ENVELOPE - ACCENT FINISH -REFER TO STYLE GUIDE

ENVELOPE - ACCENT FINISH -REFER TO STYLE GUIDE

SIGNAGE --REFER TO EXTERIOR SIGNAGE SPECIFICATIONS

TAMPER RESISTANT, RECESSED HOSE BIB - MOUNT TOP AT +12" A.F.F.

GLAZING. SEE SCHEDULE FOR GLASS TYPE.

SECTIONS FOR INFORMATION

NOT USED

FINISH CONTROL JOINT

PAINTED LINEN CHUTE, COLOR TO MATCH ADJACENT ENVELOPE

DRIER EXHAUST VENT -- COLOR TO MATCH ADJACENT MATERIAL

LIGHT FIXTURE --REFER TO CEILING PLAN

KEY NOTES:

NOT USED NOT USED

EXPANSION JOINT @ FLOOR LINE AND WINDOW HEAD W/ BACKER ROD AND SEALANT ALUMINUM LOUVER -- COLOR TO MATCH ADJACENT MATERIAL MATCHING ADJACENT

BUILDING HEIGHT DIMENSIONS ARE BASED ON WOOD FRAME CONSTRUCTION. THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION.

INTERIOR CEILING HEIGHT DIMENSIONS ARE CRITICAL TO MAINTAIN AS MINIMUMS. CONCRETE PAD - REFER TO AREA DEVELOPMENT PLAN

ACCESS OPENING INTO BEACON

ALUMINUM GUTTER AND DS. COLOR TO MATCH ADJ. MATERIAL

ACCESSIBLE KEY CARD READER WIRED TO ELECTRIC STRIKE IN DOOR - TOP OF READER AT 48" MAX. HEIGHT

POOL PERIMETER FENCE AS REQUIRED BY CODE W/ 3'-0" GATE WITH PANIC/EXIT

PRECAST COPING

HI-LO DRINKING FOUNTAIN

EMERGENCY PHONE

ADA COMPLIANT STANDING SHOWER. PROVIDE TILE WALL AT SHOWER

FINISH KEY: EX | 1 | ENVELOPE - ACCENT METAL - ALUMINUM COLOR WHITE EX 2 NOT USED EX 3 ENVELOPE - WRAP - EIFS, COLOR: BENJ. MOORE AC-22, NANTUCKET FOG EX 4 ENVELOPE - LINK & KEEP - EIFS, COLOR: BENJ. MOORE HC-70 VAN, BUREN EX 5 ENVELOPE - ACCENT - EIFS, COLOR: BENJ. MOORE 2111-50, STONE HARBOR

EX 6 ENVELOPE - CORE - EIFS, COLOR: BENJ. MOORE 2109-40, SMOKED OYSTER EX 7 ENVELOPE - SUPPORT - CAST STONE. ARRISCRAFT RENAISSANCE - SERIES GINGER-SMOOTH FINISH

EX 8 ENVELOPE - SUPPORT - CAST STONE. ARRISCRAFT RENAISSANCE - SERIES GINGER-ROCKED FINISH

EX 9 BEAM: VERTICAL ELEMENT SET BACK IN PLANE FROM THE KEEP, EXTENDS TO THE GROUND. COMPOSED OF THREE ELEMENTS

EX 9A REVEAL: VERTICAL ELEMENT EXTENDING THROUGH THE KEEP, EIFS, COLOR: BENJ. MOORE 398, FLOWER POWER EX 9B BAND: HORIZONTAL ELEMENT CAPPING THE REVEAL

EX 9C GLASS CROWN MONOLITHIC ELEMENT RESTING ON THE BAND, INTENDED TO BE VISIBLE FROM THE SITE AND BEYOND. TRANSLUCENT WHITE GLASS OR POLYMER, INTERNALLY LIT WITH MINIMAL, UNOBTRUSIVE STRUCTURE. EX | 10 | ENVELOPE - CANOPY - STEEL, COLOR: BENJ. MOORE OC-17, WHITE DOVE

EIFS, COLOR: BENJ. MOORE 398, FLOWER POWER

EIFS TYPE/THICKNESS NOTE: INDICATES EIFS NUMBER WITH INDICATES EIFS NUMBER WITH

CORRESPONDING COLORS SHOWN ABOVE
INDICATES EIFS THICKNESS. IF NO NUMBER IS
SHOWN, THE EIFS IS 2" NOMINAL THICK

BUILDING SIGNAGE:

• ALL SIGNAGE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. LOCATION SIZE, ETC. TO BE DETERMINED ON AN INDIVIDUAL PROJECT BASIS. • MINIMUM 3/4" PLYWOOD BACKER-BOARD REQUIRED AT SIGN LOCATIONS. AREA SHOULD COVER ENTIRE LENGTH AND HEIGHT OF FASCIA OR SPACE AVAILABLE FOR SIGN. • ELECTRICAL AND FINAL CONNECTION BY CONTRACTOR. ELECTRICAL REQUIREMENTS MAY BE OBTAINED FROM SIGN COMPANY. TYPICAL 120V REQUIREMENT.

• RACEWAYS/ WIREWAYS ARE NOT ALLOWED. PERMANENT ACCESS DOORS TO INTERIOR OF ALL PARAPETS WHERE SIGNS ARE LOCATED TO BE PROVIDED BY CONTRACTOR. CONTRACTOR TO FURNISH AND CONNECT PRIMARY ELECTRICAL SERVICE INSIDE PARAPET WALL. GUESTROOMS OR PUBLIC SPACES WITH SIGNAGE ON EXTERIOR WALLS MUST HAVE ELECTRICAL PENETRATIONS AND/ OR INSTALLED PRIOR TO CLOSING UP THE WALL. PENETRATION LAYOUT MA

GENERAL NOTES

BE OBTAINED FROM SIGN COMPANY.

REFER TO HOME 2 SUITES BY HILTON STANDARDS MANUAL GUIDE FOR ADDITIONAL INFORMATION REGARDING EXTERIOR MATERIALS, CONSTRUCTION REQUIREMENTS AND SIGNAGE REQUIREMENTS.

GROUT COLOR TO MATCH EXTERIOR TILE OR MASONRY.

BUILDING HEIGHT DIMENSIONS ARE BASED ON WOOD FRAME CONSTRUCTION. THESE DIMENSIONS WILL VARY BASED ON FINAL BUILDING CONSTRUCTION. INTERIOR CEILING HEIGHTS ARE CRITICAL TO MAINTAIN AS MINIMUMS.

EIFS FINISH SYSTEM OVER MIN. 2" NOMINAL THICK EPS ON EXTERIOR SHEATHING AND APPROVED BUILDING WRAP. APPROVED MANUFACTURER PER HILTON SPECS - "WATER MASTER DRAINABLE SYSTEM" OR EQUAL TO BE USED. SEE MANUFACTURER'S SPECS. FOR NSTALLATION DETAILS.

ENLARGED ELEVATION

BARCLAY WEST TRACT B

WILMINGTON, NORTH CAROLINA

PAPER SIZE: 24X36 (ARCH D) SCALE: AS NOTED

SHEET NUMBER: